

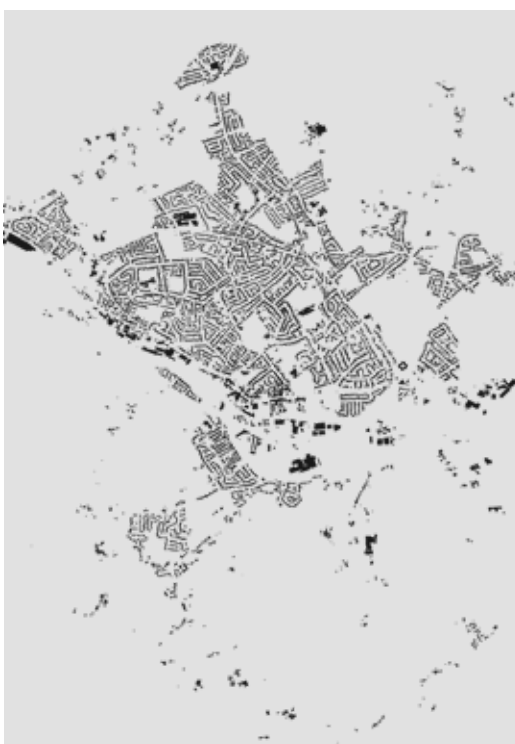
MIRFIELD

Neighbourhood Plan

DESIGN CODES

Quality information

Document name	Ref	Prepared for	Prepared by	Date	Reviewed by
Upper Hopton Design Guide	DR-10935	Mirfield Neighbourhood Plan Steering Group	Nick Beedie, Elliot Jodrell, AECOM	18.02.21 03.03.21	Christine Sykes - Mirfield (NPSG) John Wilkinson (Locality)



Mirfield Figure-ground

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1. INTRODUCTION

This document describes Mirfield as it is today, and highlights the qualities valued by its residents. It incorporates local knowledge, views and ideas supported by a great deal of research on the part of a small dedicated community driven team. Their aims are based on an understanding of past and present, thereby contributing to a positive future.

1.1 Background

This is a large neighbourhood plan area with a sizeable housing requirement of 400 dwellings (allocated through the local plan). There is an extensive area of Green Belt which goes very close to the built-up area, there are significant flooding issues along the River Calder and the existing urban areas are very dense with little potential for major new development.

The neighbourhood area is broadly characterised by two distinct areas, the north of the neighbourhood area is urban with an historic core (an area that is potentially suitable for a Conservation Area). The south of the neighbourhood area is largely rural with some small villages, one of which (Upper Hopton) has a Conservation Area and rural village character.

As this Neighbourhood Plan is not allocating sites there is no requirement for site specific master planning. However, there is a need for at least two separate design codes for the

different areas and therefore a complex level design code package has been delivered.

1.2 Purpose & Scope

The overarching aims of this Design Code are to promote locally distinctive new development and sensitive redevelopment in Mirfield that builds from the historic character that gives the Parish its sense of place. Many recent developments have attempted to be sympathetic to their historic context but there has also been much sprawling suburban development that has enveloped the historic patterns over time.

The buildings, streets and spaces of the parish must be designed to be beautiful, give a sense of place and follow traditional settlement logic and building vernacular or innovate with purpose to deliver contemporary high-quality architecture that is highly sustainable.

This report provides the evidence base for the Mirfield Neighbourhood Plan policies on design and development.

Who should use it:

This Design Code is addressed to statutory bodies, planners, developers, local interest groups, businesses and residents and anyone else with an interest in the urban environment including as a resource for schools and marketing.

1.3 Aims & Objectives

The following objectives were derived from inception calls with the Neighbourhood Plan team and their planning consultants:

1) Assess the parish area and conservation area:

- Analyse the spatial pattern of the Parish and its various settlements, including the following frameworks; Movement structure, Landscape structure, Neighbourhood structure; and Townscape & Heritage,
- Assess the different character areas of Mirfield Parish's landscape and built-form in a cross-cutting analysis of locations (e.g. Town centre, Urban, Suburban and Semi-Rural village);

2) Protect heritage and promote character across the whole parish area:

- Focus on historic areas, characteristics and features that make Mirfield special;
- Promote and reinforce these with design principles, guidelines and codes;

3) Encourage the right types of development in the right place:

- Promote locally distinctive models for new development and regeneration;
- Focus on how heritage can be used as a catalyst and focus of regeneration;

1.4 Methodology & Engagement

Due to Covid19 restrictions this technical support package has largely been delivered remotely together with regular online meetings with the Neighbourhood Plan Group via Zoom. A site visit was undertaken on (22.9.20) and a broad cross-section of the parish, particularly urban areas where development would be likely to take place, was assessed / photographed.

The illustrative mapping of the Parish was produced using OS mapping provided by the local authority under end user license agreement and with reference to other sources of online mapping including Magic Map (magic.defra.gov.uk) and Google Earth Pro desktop.

The following steps, together with a monthly review meeting with the neighbourhood plan steering group have underpinned the understanding of place and local distinctiveness;

- Inception meeting with Neighbourhood Plan team and planning consultant (Yorplan);
- Solo Parish walking tour and photographic site survey (22.9.20);
- Local planning policy context and design guidance review;
- Desktop mapping for spatial frameworks of the Parish; site photographic review and desktop Characterisation Study;
- Illustration and team review of design principles, codes and guidelines; and
- Final presentation to the Neighbourhood plan steering group on (18.2.21).

1.5 Document Structure

The document is structured in five parts to reflect the stages of assessment and evidence gathering that has influenced the resultant design codes. These are:

- Introduction & objectives
- Parish Assessment
- Character Study
- Design Codes
- Next Steps

1.6 Policy and Guidance

The following includes relevant policies and extracts at both the National and local levels.

National Planning Policy & Guidance:

- **National planning policy framework (NPPF)**
The National Planning Policy Framework (NPPF) places significant emphasis upon design, stating that good design is indivisible from good planning. NPPF requires local authorities to give significant weight to outstanding or innovative designs and should refuse permission for poor design that fails to take opportunity to improve character and quality of an area and how it functions.
- **National Planning Practice Guidance (NPPG)**
The National Planning Practice Guidance (NPPG) sets out further guidance on the importance of good design, what is a well-designed place and what processes and tools can be used to achieve good design.

Local Planning Policy:

- **Kirklees Local Plan (adopted Feb. 2019)**
for summary policies see Appendix A.1

Supplementary Planning Documents and Guidance:

- **Housebuilder Design Guide SPD**
This guidance is intended for use by residential developers and aims to ensure future housing development is high quality, socially inclusive and built to high environmental standards. The document provides guidance for housing developers on working with the Council at the outset of the development process to meet our aspiration of achieving quality places.
- **House Extensions and Alterations SPD**
This Supplementary Planning Document (SPD) has been prepared to help householders, developers, agents and architects who are planning and designing an extension or alterations (householder development) to an existing residential property, including conservatories and outbuildings, such as garages. It provides detailed guidance regarding the standard of development that will help achieve a well-designed house extension or alteration required by the Council.
- **Open Space SPD**
The Open Space SPD provides detailed guidance for applicants and the local community on the requirements for open space, sports and recreation provision to serve new housing developments. It sets out a five step-by-step approach to be used in determining the types, amount and location of open space required to serve new housing developments across Kirklees.

- **Biodiversity Net Gain Technical Advice Note**

The draft Biodiversity Net Gain Technical Advice Note provides clarity for applicants on how to achieve biodiversity net gain through development within Kirklees and supports national and local legislation. It sets out clear guidance on how biodiversity should be considered throughout the development process, including the utilisation of Defra's Biodiversity metric to demonstrate a 10% net gain in biodiversity which is due to be mandated by the Environment Bill.

- **Kirklees Historic Landscape Characterisation Project (2017)**

- **Upper Hopton Draft Conservation Area Appraisal**

- **Mirfield Design Statement (2002)**

Other guidance and supporting evidence:

- **Secured by Design**

Sets out detailed considerations for how the design of new build homes can help reduce the opportunity for crime and the fear of crime.

- **Manual for Streets 1 & 2**

- **The National Design Guide**

- **Building for a Healthy Life (2020)**

Sets out a checklist for how good design can be achieved in residential schemes. To be read in conjunction with the original Building For Life 12 document.

- **Sport England Active Design Principles**

The design of development can have positive impacts on health and well-being. The Sport England Active Design Principles provides guidelines for how the design of the built environment can increase physical activity

1.7 Local Context

Kirklees District

Kirklees is a diverse district, encompassing a range of development styles and types from different eras. Several hilltop settlements have medieval origins, whilst the valleys and main towns were developed from the industrial revolution.

Development in the latter half of the last century was focused around large industrial [expansion] and car-dominated residential [estates]. From the end of the last century, to now, former industrial land in the middle of settlements has been re-used for residential, leisure and retail development.

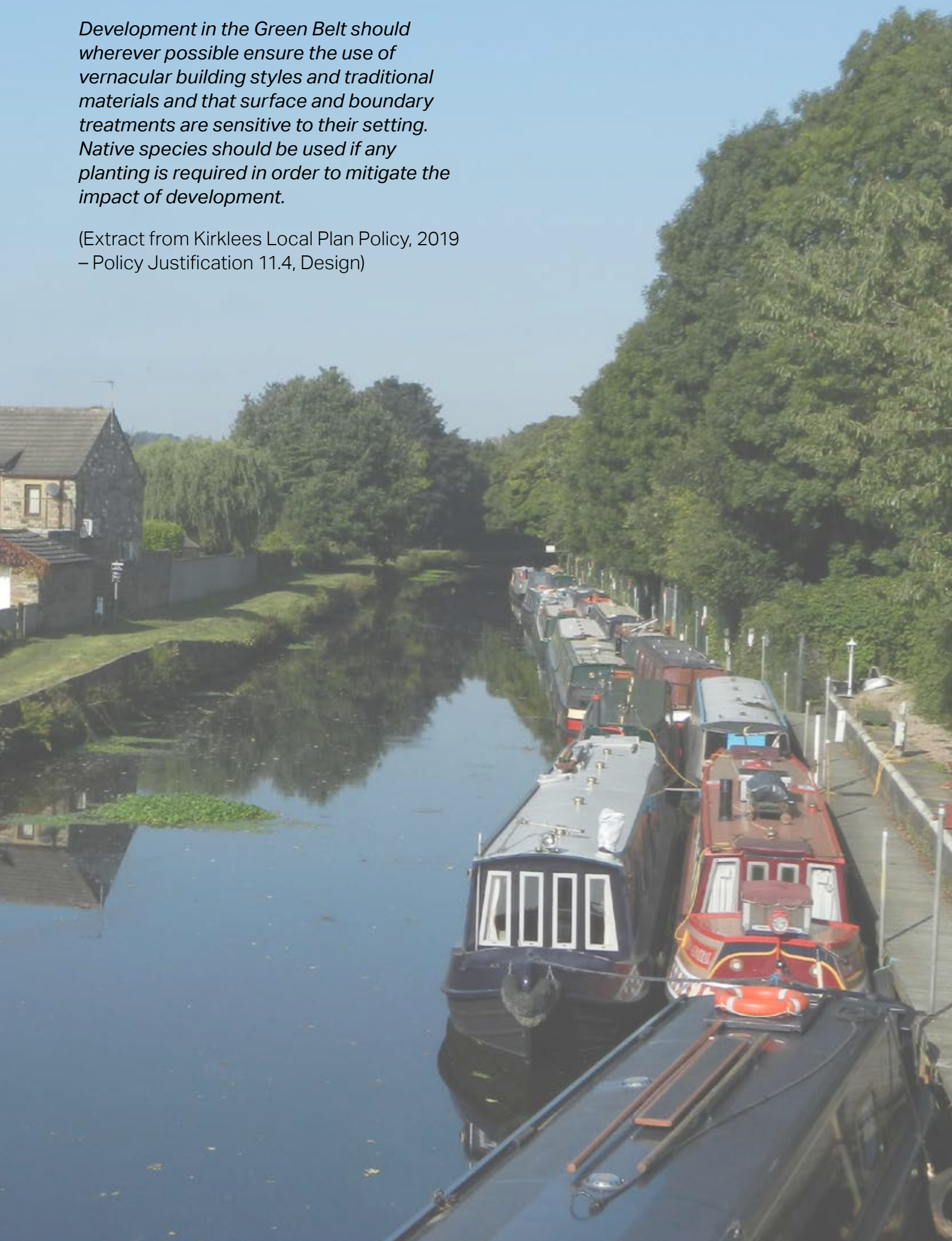
Kirklees has more heritage assets than any other local authority in Yorkshire. Many of these are intrinsic to the character of the townscape, further information is in the policy on the Historic Environment.

The canal network across the district is a legacy of the industrial revolution; new development can help to enhance its setting and support the recreational role that it has today. The topography across much of the district, particularly towards the Pennines in the west of the district, means that views and vistas should be given particular consideration, especially towards the Peak District National Park.

The diverse built and natural environment in the district presents a range of different challenges, requiring bespoke solutions to help respect and enhance character, particularly within conservation areas and when development proposals may impact on the setting of listed buildings. The urban areas of the district are surrounded by Green Belt where national policy states that local planning authorities should plan positively to retain and enhance landscapes, visual amenity and biodiversity.

Development in the Green Belt should wherever possible ensure the use of vernacular building styles and traditional materials and that surface and boundary treatments are sensitive to their setting. Native species should be used if any planting is required in order to mitigate the impact of development.

(Extract from Kirklees Local Plan Policy, 2019
– Policy Justification 11.4, Design)



2. PARISH ASSESSMENT

2.1 Neighbourhood Plan Area Overview

Mirfield is a town and civil parish in Kirklees, West Yorkshire (formerly part of the West Riding of Yorkshire historically). It is approximately 6.7km northeast of Huddersfield. It is on the A644 which links between Brighouse and Dewsbury (a main district centre). At the 2011 census it had a population of 19,563. The town is served by Mirfield railway station and straddles the River Calder, up both slopes of the Calder Valley but more densely to the north. Almost two-thirds of the plan area is designated Green Belt. Several housing and mixed-use sites are allocated within the area. To the East, mostly outside of the NDP boundary, is a large allocation of housing for up to 4,000 homes (Dewsbury Strategic Riverside Site). The village of Upper Hopton is a designated Conservation Area also.

Takeaway:

The area includes a range of situations: strategic road, river and canal corridors; Green Belt inc. woodlands and rural land; the town centre, employment areas and residential neighbourhoods. The design codes that follow are a response to all these situations in varying degrees of detail and in line with local plan policy.

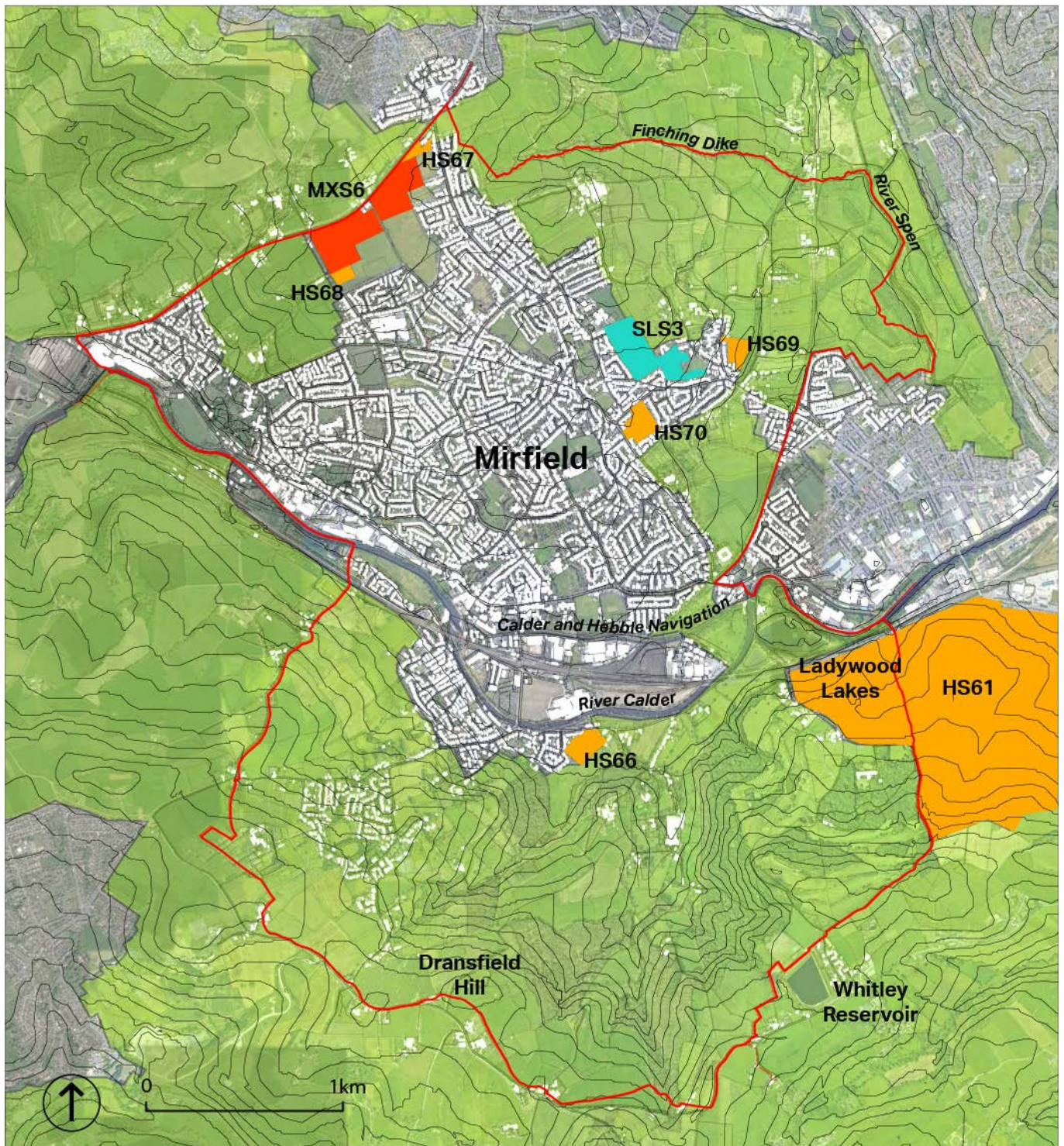
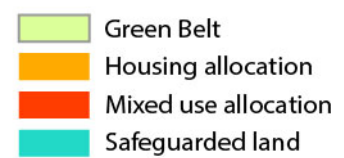


Figure 1: Study Area



2.2 Historic Growth

Located within a significant former coal mining area and various other industries including wool textiles, agriculture and malting (processing malt for food and drink) played a part in its history. Likely an agricultural settlement originally (e.g. Upper Hopton), the various villages expanded significantly during and after the industrial revolution and changed into a sprawling developed area with the addition of housing developments and modern business activity / diversification.

- Originally Mirfield will have been covered by forests that were compacted over many thousands of years and compressed to coal which, once found, triggered a lucrative mining industry in Mirfield through C19 and early C20.
- In 1871 a first church had been built and consecrated in Mirfield. The tower still stands next to the parish church (itself a highly visible local landmark).
- In C14 Mirfield was a prosperous centre for the woollen industry, based mainly around textiles, but it was already a place of some agricultural and industrial importance and later the emphasis moved towards mining and malting.
- Before the late 1960's Mirfield was the site of a busy locomotive depot and sorting sidings covering a great area and with much activity. Closure of the Battyeford Station and the Leeds New Line impacted the town as part of national railway saving strategy.
- The River Calder has been the life blood of the town from fishing (and even skating) to the movement of resources on the Victorian constructed Calder and Hebble Navigation. This proximity and takeover by industry of an ecological corridor comes at an environmental and biodiversity cost (will need to achieve biodiversity net gain now).
- Various quarry sites and slag heaps associated to the mining industry have long been shut down and some sites were built on and/or redeveloped for new housing schemes.

Takeaway:

By looking at the historic fabric, it is possible to find clues, design motifs and uncover the unique character of every part of Mirfield. This must guide any future development and the growth of the town, in order to develop a strong 'sense of a place' that is rooted in its origins and key periods.

In this way the existing urban fabric is a design resource to be used to identify precedents for new development of all types that are in keeping with their surroundings and add to the town's history.



Figure 2: Historic OS Map Extracts (1892)



Figure 3: Historic OS Map Extracts (1948)

2.3 Landscape Context

Open spaces and Public Rights of Way

There are a variety of types of open spaces distributed across the Neighbourhood Plan area including; sports pitches, school fields, parks and recreation grounds, allotments, church grounds and smaller amenity green spaces. Linear spaces such as the riverside and canal towpath are also valuable connecting 'green and blue' spaces running along the valley bottom. Dewsbury District golf course is on the hillside towards the south-east of the area and incorporates several public rights of way,

There are many more public rights of way traversing the valley slopes and (in combination) following the valley bottom, including the Calder and Hebble Navigation towpath. Upper Hopton is connected to Lower Hopton and there are sections of footpath throughout the northern body of the town which connect between neighbourhoods and historic open spaces, alongside the extensive street footpath network. There is excellent coverage of footpaths around the woodland to the south-east of the Neighbourhood Area and throughout Green belt areas, giving good access to the countryside from the urban areas.



Takeaway:

Existing open spaces identified in the Neighbourhood plan are protected from development. All new spaces as part of development must be accessible, safe, overlooked and strategically located within the site and well-integrated into wider green infrastructure networks to contribute towards enhancement of the natural environment, support biodiversity and connect to and enhance ecological networks and green infrastructure.

Retention of valuable or important trees and planting of new trees and other landscaping will be necessary to maximise visual amenity and environmental benefits. Existing views to the countryside and notable landmarks must be retained and enhanced. Development must not contribute to increasing flood risk either for itself by its location or other land due to its form and siting.

Figure 4: Photographs: Upper Hopton; Calder and Hebble Canal path; Recreation ground



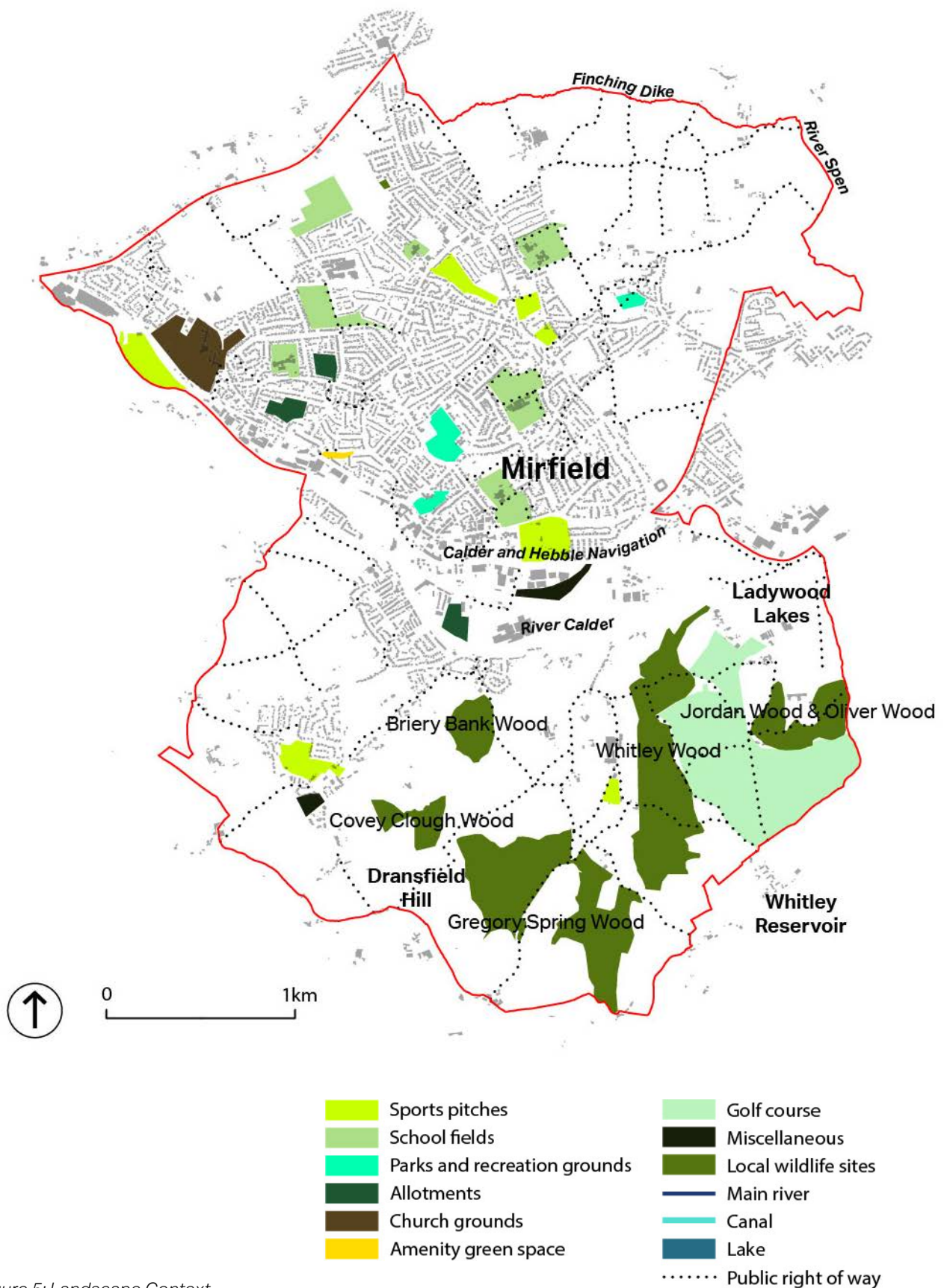


Figure 5: Landscape Context

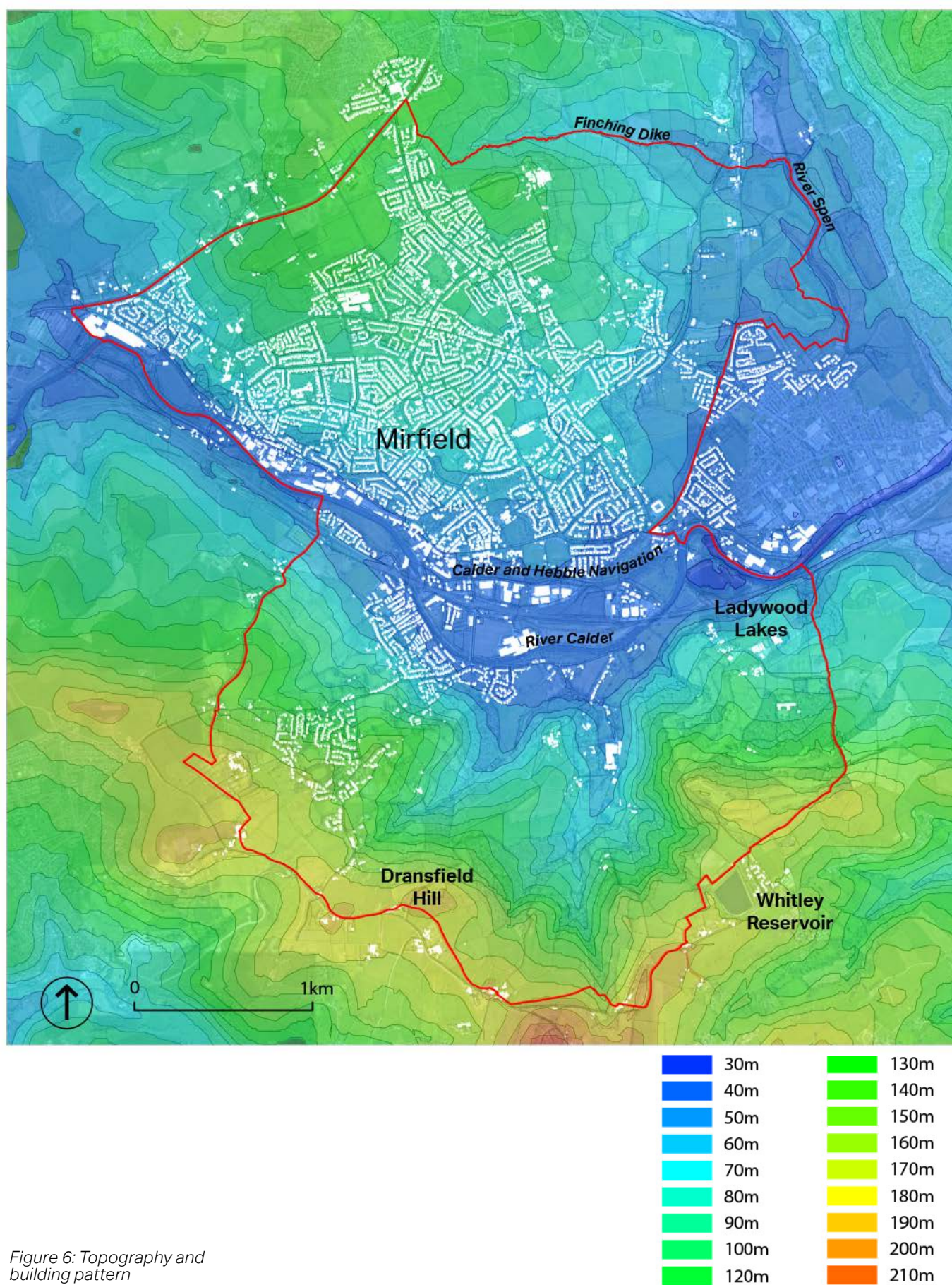


Figure 6: Topography and building pattern

Topography, flood risk and views

The town is sited within the Calder Valley on the eastern slopes of the Pennine foothills. Open spaces (e.g. church yards, recreation fields, school pitches), riversides, tow-paths and streets allow views across the town and out or along the Calder Valley and to countryside. It was once said that you can see a field from every street in Mirfield... no longer the case but gaps between buildings allow many glimpsed views out to the countryside and hills which provide an important link between the town and its rural hinterland.

The topography and hydrology of the Calder River valley is integral to the settlement pattern of Mirfield's settlement and indeed it's origins and growth in terms of water powered mills and industry. The valley bottom is naturally at risk of flooding but this area is fairly limited in Mirfield due to the steepness of the valley sides and is predominantly kept free of residential development but allowed for industrial uses. Flood defences are also in place.

The moderately sloping hillsides north of the river influence the pattern of routes, streets and spaces and require the alignment of homes and buildings to take significant slopes into account in places. South of the river the slopes are steeper and likely part of the reason this side is less developed. Solar orientation also favours the sun facing northern side of the valley.

- **Woodland:** Some woodland cover remains on the hillsides south of the river, which was a previously largely wooded area, some are classified as Ancient Woodland. Predominantly in the south / south-east are Covey Clough Wood (predominantly deciduous trees); Briery

Bank, (deciduous trees and scrub), Gregory Spring Wood, Whitley Wood Wood, Jordan Wood and Oliver Wood.

- **Watercourses:** The River Calder rises on Heald Moor in Lancashire and then flows east into West Yorkshire through green countryside, former woollen-mill village and large and small towns (including Mirfield) before joining the River Aire near Castleford. The Calder and Hebble Navigation is a broad inland waterway, with locks and bridgeholes that are suitable for 14-foot-wide (4.3 m) boats. The Navigation starts in Wakefield, where there is an end-on junction with the Aire and Calder Navigation and runs upstream through Mirfield, after which there is a junction with the Huddersfield Broad Canal, to arrive at Sowerby Bridge, where it meets the Rochdale Canal.

Takeaway:

It is key to understand the character of the landscape in order to ensure that any new building is appropriate to its context and respects the character of the Calder Valley. If a development's design is responsive to local landscape character and the unique qualities of a site then it is more likely to contribute to a locally distinctive place and be appropriate to its setting. Take steps to ensure that a building ties into the local landscape and does not detract from key views. Also, it follows that buildings in one area of the Parish may not be appropriate in another different area.

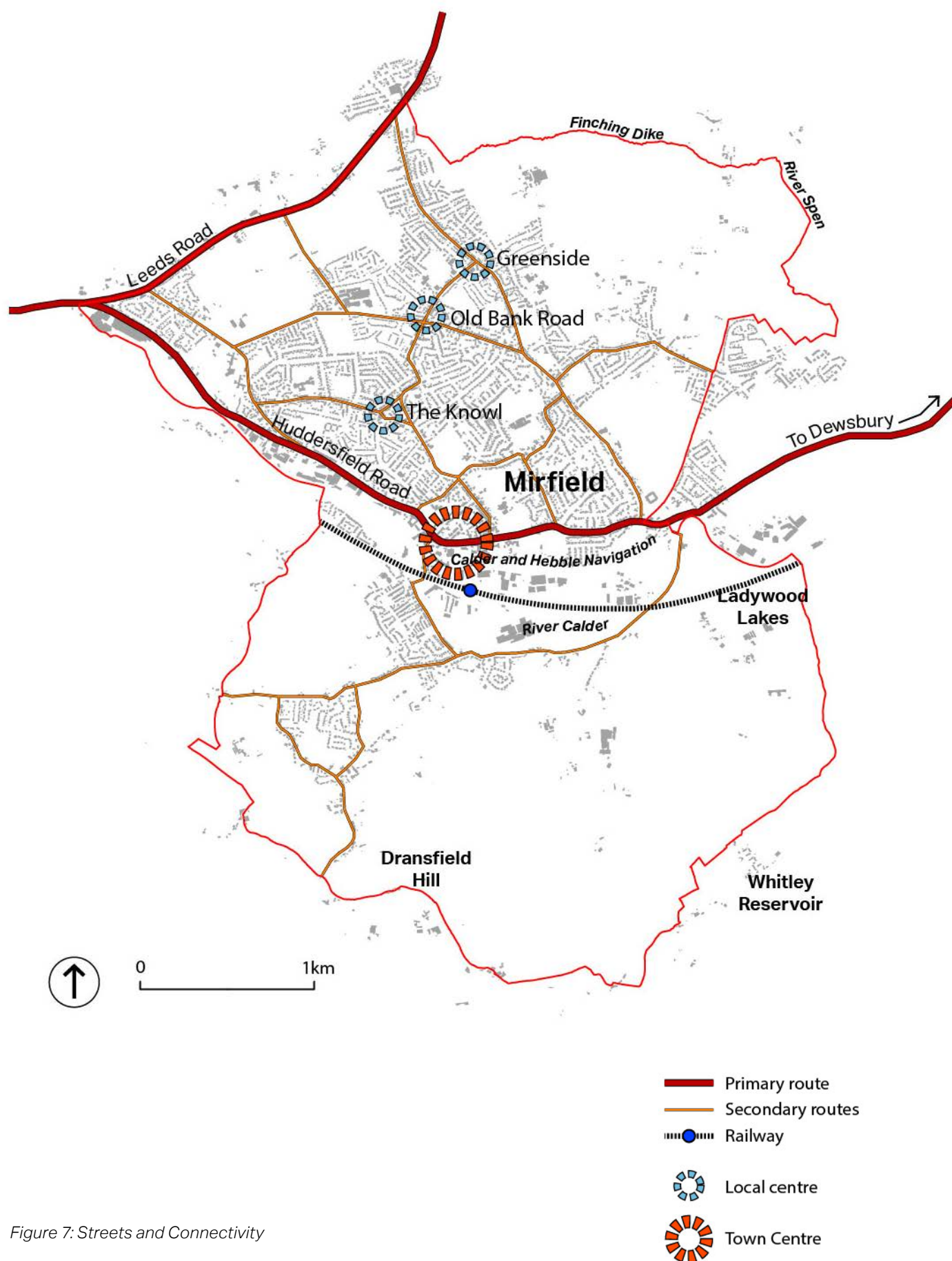


Figure 7: Streets and Connectivity

2.4 Strategic Movement

This section outlines key the strategic movement routes and entry points.

Rail: Mirfield has a very basic train station, which consists of not much more than an exposed pair of platforms; however, it benefits from direct trains to Leeds, Huddersfield, Manchester and London.

Bus: Longstaff's, a private company operates the 205 between Mirfield & Dewsbury via Northorpe & Ravensthorpe (similar to the 202). The 261 goes from Mirfield to Cleckheaton via Roberttown, Liversedge & Heckmondwike. The 262 from Huddersfield to Mirfield goes via Kirkheaton and Upper Hopton. The National Express to London calls twice daily and stops at major cities en route. Road links to Wakefield & Halifax are covered by National Express.

Footpaths and Bridleways: Mirfield has some 80 footpaths, with a total length of 7km (Mirfield Design Statement).

Cycle Routes: A strategic cycle route the 'Calder Valley Greenway' passes through Mirfield. The route runs from the Colne Bridge as National Route 66, taking you all the way to Dewsbury via Mirfield. The current route passes through the town centre and a new route is being consulted on that will use quieter paths or streets, avoiding the Huddersfield Road within the town centre.

Roads: The busy Huddersfield Road (A644) passes between the Leeds Road (A62) to the east, through Mirfield, to Dewsbury to the west (following the former Turnpike Road). This route passes directly through the town centre of Mirfield as a high street. The A62, which connects Huddersfield with Leeds forms the north-west boundary of the area.

Bridges and gateways: Cooper Bridge (Leeds Rd); Wood Lane Bridge (to Hopton Lane); railway bridge (train only); Ledgard Bridge; Hopton New Road; Steanard Lane.

Street hierarchy

The secondary street pattern of connecting neighbourhood streets north of the River Calder criss-crosses the hillside, often following historic field / estate boundaries and lanes and that previously connected the historic settlements, estates and farmsteads. Church Lane forms a boundary to the east and also connects west across the Hillside via Dunbottle Lane, Green Lane and Kitson Hill Road. To the south of the river, Calder Road follows the River from Newgate (crossing to the town centre) until it meets Hopton Lane which connects Upper and Lower Hopton. Granny Lane/ Steanard Lane connects the outlying properties and estates further east along the River Calder.

Takeaway:

It is important to understand the wider strategic movement as these links can drive placemaking through the accessibility of a variety of modes of travel, whilst reducing reliance on vehicular modes. Streets are the essential structure of a town and have a place and movement function. Therefore, the movement hierarchy as well as the modes of travel, are essential when it comes to streetscapes and character of the place. Historic routes must be protected and valued.



Canalside footpath and public realm improvements

2.5 Neighbourhood Structure

The parish area is characterised by two broad areas, the north of the parish area is urbanised with an urban core (the town centre and riverside) and several small local centres distributed throughout the suburban housing that has *spread like gravy over a tablecloth* (to borrow a phrase from George Orwell) north of the Huddersfield Road.

The south of the neighbourhood area is largely rural with some small villages, one of which, Upper Hopton has a village centre with a small number of community facilities and services. Lower Hopton is more traditional and urban residential in nature (attached buildings) and fronts the river.

The northern and southern sides of the settlements are divided by several channels, barriers and strategic movement routes (as outlined in the previous section) following the valley-bottom, including: the Calder and Hebble Navigation, Leeds-Manchester railway line and the River Calder itself

Outlying farmsteads and hamlets are also situated within the Green Belt in more isolated locations, enjoying a more rural character and being associated with the valley's farmlands.

2.5.1 Land Uses

The predominant urban land use across the Parish area is residential, mostly suburban in nature with a smattering of historic centres, schools, community facilities and churches in amongst. The two principle residential areas are north of the Huddersfield Road (suburban) and south of the River Calder including Lower Hopton (urban) and Upper Hopton (semi-rural).

The northern residential area spreads north from the linear, mixed-use Town Centre that is formed along the Huddersfield Road. The main employment areas are strung-out along the valley bottom (flood plain) and sandwiched between the River Calder and either the Calder and Hebble Navigation (canal) or the Huddersfield Road. Some residential redevelopment occupies former industrial sites, quarries and former mill buildings along this corridor now.

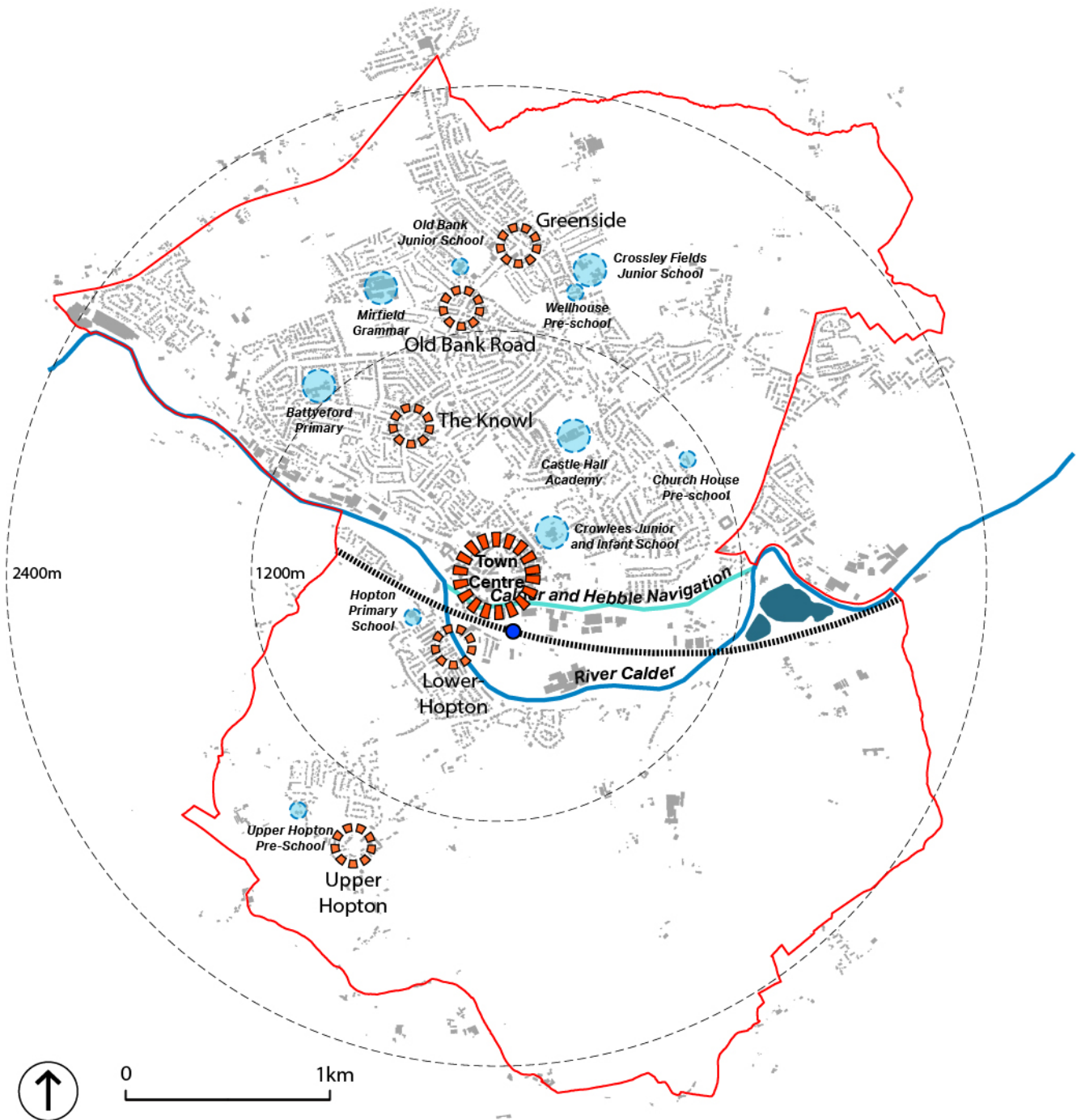
2.5.2 Walkable neighbourhoods

2.6 The walkability of the town is important and will certainly contribute to an active and healthy lifestyle based on the sloping topography. This must be in line with other modes of accessibility such as wheelchair use and cycling which become challenging on steeper slopes, both north and south of the River Calder. Facilities and particularly green space must be accessible to all.

Takeaway:

Recommendations for the design of new and existing Neighbourhoods:

- The aspiration should be for everyone to live within five minutes walk of a significant green space or park, and never more than 10 minutes.
- The aspiration should be for everyone to live within five minutes walk of a basic range of local facilities, including shops, and never more than 10 minutes.
- Homes, facilities, and green spaces should be linked by connected, walkable, and green streets and by high quality walking and cycling infrastructure with, wherever possible, low levels of traffic.



The principles of the '15 minute neighbourhood' require that access to basic needs and services within 15 minute radius to improve the daily lives of all local residents and as a response to the issues highlighted during the pandemic, e.g. significant dissatisfaction with newer neighbourhoods that don't provide these daily needs (for detailed evidence see the 'Home Comforts' report by Place Alliance, 2020).


-  Railway
-  River
-  Canal
-  1200m walk bands (from Town Centre)
-  Schools
-  Sub-centres
-  Town Centre

Figure 9: Centres, Schools and Indicative 1200m Walk Bands (approximately 22-28 mins accounting for descent / ascent)

2.5.3 Townscape & Legibility

The townscape, that special structure and form of the town is most important in the town centre but has also been eroded in places so that it no longer reads as coherently as it could. Important buildings such as the fire station may be fit for purpose but the scale and modern style pays no regard to the richness and 'rules of assembly' that have been used before and so it forms a poor end view to a key vista from Knowl Road. Where the buildings in Mirfield depart from the past completely is often where the town 'falls apart' somewhat and this is because buildings should not be designed in isolation from one another, or in isolation from the past layout which often responds to the very landscape and layers of history below it.

Centres & identity

The town of Mirfield has grown up around several small settlements, some which are villages in their own right. There are therefore several self-contained centres clustering around the nucleus of the Town-centre at Eastthorpe. The town centre exhibits much high quality building that has endured from its historic periods of growth with important, grander buildings forming a high street that signifies the historic identity and enduring commercial focus of the settlement. The buildings remaining give a sense of the special place and importance of Mirfield historically and it is vital that new buildings continue to contribute to the pattern because it has endured with many timeless qualities.

The simple endurance of it is a testament to sustainable building form that is also built to last and can be adapted, and has been, over time. Townhouses in the centre have provided homes, offices and shopfronts that have changed but mostly been fit for purpose to this day still.

[see Figure 8 for centres locations]

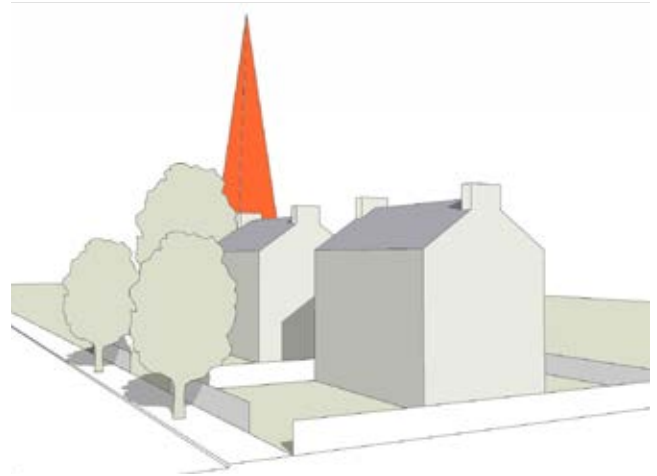
Definitions:

Townscape: The term 'Townscape' refers to the overall character and composition of a town, including the buildings streets, spaces and details. Gordon Cullen, who pioneered the concept proposed it as, 'the art of giving visual coherence and organization to the jumble of buildings, streets and space that make up the urban environment'. As such, we are able to undertake an urban visual analysis to assess the degree to which these aspects are present, providing stimulation and a setting to urban exchange and interactions – an urban social life.

Legibility: The term 'Legibility' refers to the physical and spatial characteristics of the environment, including both visual and non-visual sensations of colour, scene, motion, smell, touch and sound. All are all cues for orientation within the urban environment that reinforce legibility.

Variety & richness

We must use the lessons of the past, a strong connection to the landscape and topography in town building to create meaningful and legible environments that delight and inform the user about use, location and importance. Beyond the more functional and aesthetic aspects there should also be some scope for variance and unusual delight from areas that can be discovered more deeply over time. Mirfield has these layers of history and pieces of the past to build around but cannot afford to lose any more of this character and future buildings must add to and repair the townscape, continuing the story. Of vital importance is that landscape character must also be recovered and knitted into this structure, not simply built over.



Takeaway:

A comfortable, stimulating and legible Townscape are qualities that all new development should help to promote, whether it be background street terraces, a side alley shop or a key focal building, it will help to generate an interesting and 'legible' place that is attractive to people and helps to generate community interaction and exchange, maximising the attractiveness for activity and serving local shops and businesses with footfall.

Key streets & thoroughfares

The terraced cottages, built in a linear pattern along the lanes and thoroughfares of the town, remained dominant. This forms the most commonly seen face of the village and these routes are most important to reinforce this historic character, workmanlike though it may be.

Requiring short development setbacks for new terraces or front boundary walls to define plots on key streets can help to continue the sense of a 'joined-up' built form that characterises the historic street-scenes.

Edges and links to the countryside

Legible green links to Mirfield's many public rights of way (see figure 16) provide a relief to the townscape, enabling exploration and leisure walking within the valley and beyond and this requires comfortable, accessible routes with obvious gateways to them.



Figure 10: Strip of Traditional buildings type examples: Mills and workshops, former work-houses; Churches, Chapels; Shops & offices (townhouses); schools, libraries, pubs, sports pavilions; Halls/manor houses; detached houses; Terrace housing; Farmhouses/ agricultural;



Figure 11: Strip of modern building types; Supermarkets, Bungalows, semi-detached house types, detached house types, large school complexes, apartments blocks, conversions of mills and farm-houses, petrol stations, library, fire station, health centre, warehouses



Figure 12: Photographs of building alignment to the street, usually either parallel to the street or set at 90' (often depending on topography and length of building). Working with the topography is often more efficient but stepped buildings also help to read the levels whilst providing 'eyes on the street'.

2.7 Built Form and Heritage

2.7.1 Building types

The traditional buildings are a mixture of stone-built houses, halls, barns, terraces and cottages sometimes with secondary outbuildings, stables or garages. Buildings are typically up to two-storeys. The great majority of buildings are residential with some civic / community uses amongst them. The town centre is much more diverse in its uses with a range of shops, services and facilities as well as residential streets knitted close around it (St. Paul's Road and Knowl Road). In the outlying historic focus areas wide fronted stone terraces are mixed in amongst modern infill development but often hold key vistas as they were aligned to changes in direction of the lanes/field pattern. Narrower terraces sometimes at 90° to the street with gable end capping the view. Terraces and detached buildings (former halls or farms) are sometimes hidden within wider 'estate' blocks with narrow lanes serving them, adding interest.

Traditional Building types:

- **Industrial** – mills and workshops, former work-houses
- **Institutional** - Churches, Chapels
- **Commercial** - shops & offices (townhouses)
- **Community** – Village halls, schools, libraries, pubs, sports pavilions
- **Halls** – former estates, many now developed (built over) as modern housing estates

- **Grander detached houses (town centre)** – 2 – 2.5 storey homes but grander in scale, e.g. in proximity to a park entrance as on Knowl Road
- **Terrace housing** - Stone terraces (industrial era) - wide fronted terraces on key frontages and narrower terraces stepping up hills
- **Farmhouses / barns** – stone houses in more isolated positions, often on hill-tops, often converted to private homes

Takeaway: Building orientation and attachment

Buildings tend to face the streets but are sometimes aligned perpendicular to them with the short end facing the street, creating a secondary frontage. As such, buildings also tend to run with the contours of the hill (parallel) or against them. Often the longer side of the building / terrace will be aligned with the contour but there are also instances of terraces / buildings stepping up the hill as streets are necessarily aligned against (close to 90 degrees) to the slope to move on up to higher ground. Topography is a key driver for layout in Mirfield.

The historic built-form is perceived as attached due to either terraces of buildings joined directly or connecting boundary walls which link the building line. In addition, where buildings are not set close to the back of pavement low boundary walls help to form a clear plot boundary to create a contained street scene that makes clear public and private space.

2.7.2 Conservation Area

Upper Hopton was originally designated as a conservation area in 1978 in recognition of its 'special architectural or historic interest. The character of which it is desirable to preserve and enhance,' now and for future generations. The area was subsequently expanded in 2007 (see plan). The conservation area contains public open space, ecclesiastical buildings and grounds, as well as residential and some former agricultural properties. It purposely excludes the modern post-war housing developments that have infilled the area altering the villages 'internal' character. See following section for additional assessment of the Conservation area.

2.7.3 Listed Buildings

Mirfield has 47 listed buildings, 5 of which are in Upper Hopton. Most surround the River Calder valley bottom or are scattered on the northern valley slopes with an isolated one at the far south of the Parish.

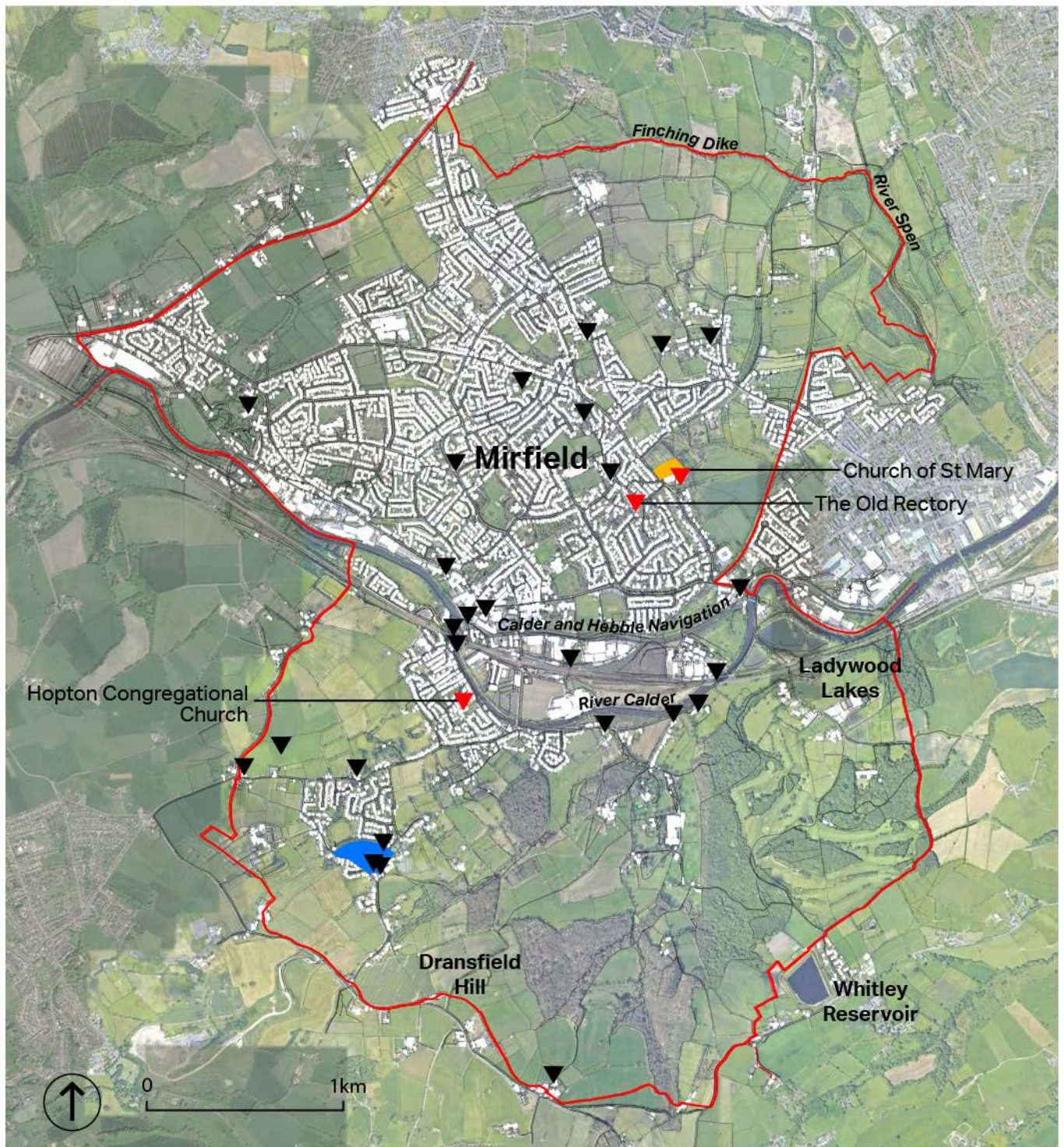
2.7.4 Scheduled monuments

Castle Hall Hill motte and bailey castle is situated adjacent to the nineteenth century parish church of St.Mary in Mirfield. The bailey is occupied by the church and its graveyard, both of which are in current ecclesiastical use, and this area is not at present included in the scheduling.

2.7.5 Buildings of Local Significance

Within the District there are buildings which, although not of sufficient architectural or historic interest to be included on the statutory list, are of local significance and need to be identified as such. Consideration will therefore be given to the inclusion of the best of the District's unlisted buildings on a 'local list', using the following criteria as a guide;

1. Examples of work by local architects or builders of esteem.
2. Buildings which are of local community interest.
3. Buildings, or groups of buildings, which contribute to the character or identity of a townscape or rural area, or which enhance a landscape.



- Castle Hall Hill motte and bailey castle
- Grade II listed buildings
- Grade II* listed buildings
- Upper Hopton Conservation Area

Figure 13: Heritage assets and building pattern

2.8 Upper Hopton (inc. Conservation Area)

2.8.1 Settlement character overview

A semi-rural hillside village overlooking the Calder Valley adjacent to Lower Hopton/ Mirfield. The village has preserved much of its historic character as well as continuing to evolve and grow over time. It remains distinctly separated from the main urban area of Mirfield.

A significant proportion of the main village streets (historic lanes) and walls / buildings that face them are protected in a Conservation Area. Therefore, the primary purpose of this Design Code is to protect and enhance this character within and around the setting of the Conservation Area.

Throughout the conservation area there are many good and historic qualities worth preserving and a scattering of listed buildings (5) across the village as well as protected trees and walls.



Figure 15: The Travellers Rest public house



Figure 14: Hopton Heritage sign in Upper Hopton

2.8.2 Local Context and settlement origins

Located within a former coal mining area and various other industries including wool textiles, agriculture and malting (processing malt for food and drink) played a part in its history. Likely a small agricultural settlement originally, the village expanded with the provision of a church by a former Malter. During and after the industrial revolution the village expanded and changed with the addition of housing developments and infrastructure to cope with the increased activity.

For further information also see Upper Hopton Conservation Area Appraisal.

2.8.3 Landscape, Topography and Views



Figure 16: Historic OS map extract of Upper Hopton (1850-51)

The village is sited within the Calder Valley on the eastern slopes of the Pennine foothills. Open spaces (e.g. church yard and recreation field) allow views across the village and out across the Calder Valley towards Mirfield / countryside. In places, the outward facing form of development (edge lanes) allow stretches of views outward (e.g. Hopton Hall Lane). Gaps between buildings allow many glimpsed views out to the countryside and hills which provide an important link between the village and its surrounding landscape. Mirfield Parish Church can be picked out as a landmark within the main urban area of Mirfield on the south face of the valley.



Figure 17: Topography, Landscape and Views

Woodland and planting

Mature trees throughout the village provide a varied setting seasonally and are key to character. Some woodlands on the nearby hillsides remain of what was a previously largely wooded area, some of are classified as Ancient Woodland. Covey Clough Wood is located to the South East (predominantly deciduous trees) and Briery Bank East of the village (deciduous trees and scrub).

Open space

There is a large amount of open space in and surrounding the Upper Hopton Conservation Area. This open space is an integral part of the character of the Conservation Area and should be preserved in order to maintain an important attribute of the village.



2.8.4 General Layout and Settlement pattern

2.8.5 The settlement is a linear village, composed around the three connecting lanes of Hopton Lane, Hopton Hall Lane and Jackroyd Lane to form a core ring (ranging from semi-open to well enclosed). This 'face' of the village is bounded by a combination of boundary walls with set back private buildings or close set buildings at the back of pavement. Breaks with hedgerows and mature trees or glimpsed views complete the streetscenes. Later additions to the village have infilled (e.g. Cheviot Way and Hopton Avenue) and some other more rural clusters have extended out from key junctions. Within this ring are the main open spaces and also community buildings off Jackroyd Lane

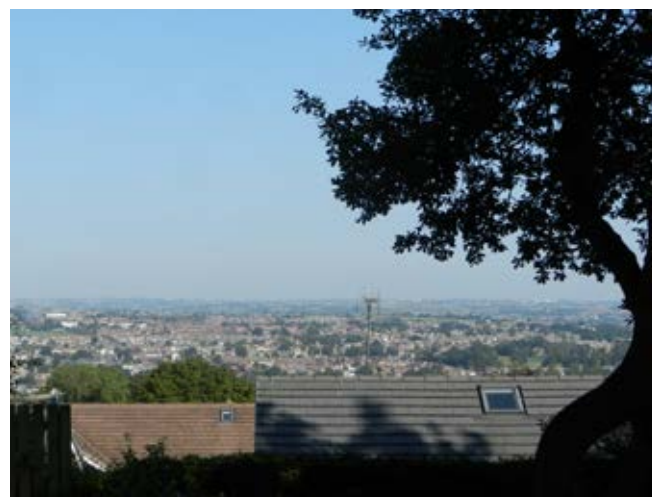


Figure 18: The western most extent of the Conservation Area nestled within mature trees and landscape



Figure 19: Plan showing Upper Hopton Conservation Area

with a key space around St. John's church, Sutcliffe Memorial grounds and also the recreation ground.

2.8.6 Conservation Area

Upper Hopton was originally designated as a conservation area in 1978 in recognition of its 'special architectural or historic interest. The character of which it is desirable to preserve and enhance,' now and for future generations. The area was subsequently expanded in 2007 (see plan).

The conservation area contains public open space, ecclesiastical buildings and grounds, as well as residential and some former agricultural properties. It purposely excludes the modern post-war housing developments that have infilled the area altering the villages overall character.



Figure 20: Church of Saint John

2.8.7 Buildings

Building orientation

Buildings tend to face the streets but are sometimes aligned perpendicular to them with the short end facing the street, creating a secondary frontage. As such, buildings also tend to run with the contours of the hill (parallel) or against them. Often the longer side of the building / terrace will be aligned with the contour but there are also instances of terraces / buildings stepping up the hill as streets are necessarily aligned against (close to 90 degrees) to the slope to reach higher ground.

Building types

The buildings are a mixture of stone-built houses, halls, barns, terraces and cottages sometimes with secondary outbuildings, stables or garages. Buildings are typically up to two-storeys. The great majority of buildings are residential with some civic / community uses (e.g. Saint John's Church, Croft House and the Pavilion building at the Cricket Grounds and the Club house). Outside of the conservation area houses are mainly detached or semi-detached 21C homes - The Travellers Rest Pub is an exception.

Boundary / street enclosure

The historic built form is perceived as attached due to connecting boundary walls which either join buildings directly or form a clear plot boundary at the back of pavement to create a contained street scene, often with minimal footpath widths and verges.

The high walls on Hopton Hall Lane illustrate the visual and physical seclusion of the detached dwellings behind them and give definition to the street layout. They provide a clear distinction between the large private spaces behind the walls and the narrow public spaces of the road.

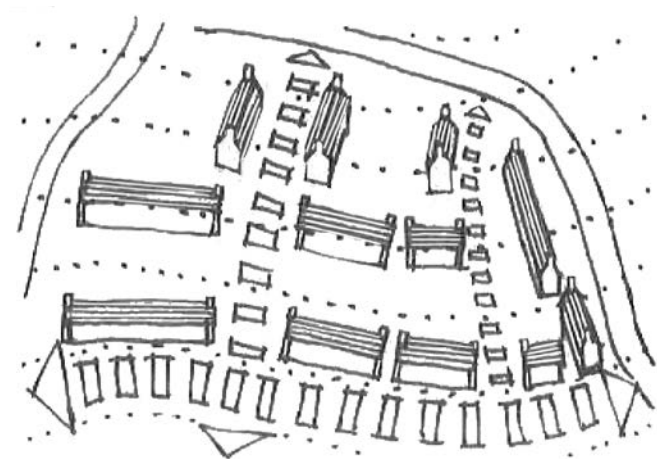


Figure 21: Building and street alignment with topography





Figure 22: Townscape diagram

Planting

Encouraging and protecting native species is paramount. This should apply to;

- Hedges;
- Trees; and
- On-plot planting

Non-native or species with low biodiversity value must be avoided



Listed buildings

Upper Hopton has 5 Grade II listed buildings.

These are:

- Main barn to Hall Farm
- Church of St. John
- Hopton Hall
- Numbers 124,126 and adjoining barn
- Northgate terraces (22-34)

Hopton Hall

Hopton Hall is a medieval moated site with the Hall situated within this feature. The Hall is a late Jacobean manor house which is surrounded by a setting of mature trees. The moat may be remains of an even earlier building. The remaining building is located on Hopton Hall Lane and is a Grade II Listed building. The structure is of a two storey house with half-timbered gable and plaster infilling. The timber framed H plan Hall was mostly constructed in the late 16th Century. The building has evolved from C15 to C19 with additional wings and materials.



Figure 23: Main barn to Hall Farm



Figure 24: Hopton Hall

2.8.8 Street pattern

The primary street pattern is formed of the above-mentioned three key lanes of Hopton Lane, Hopton Hall Lane and Jackroyd Lane. These also form three key spaces, now T-junctions around the periphery of the village. A further key T junction is found at Northgate where a formal arrangement of terraces stood around Hollin Hall. At Hopton Fold (location of St. John's Church and Hopton Hall) there is a key junction space.



Figure 25: Hopton Hall Lane



Figure 26: Street hierarchy diagram

From off of these main organic lanes come various secondary lanes and streets of more formal terraces. Some very small scale mews streets/ courts and private drives make up the overall pattern of a street hierarchy.

The Post-War trend to create suburban cul-de-sacs of semi-detached or terraced properties which was based on a poor interpretation of the garden city movement has resulted in inward facing 'pods' of development. These limit connectivity although sometimes this is not choice.



Figure 27: Hopton Hall Lane

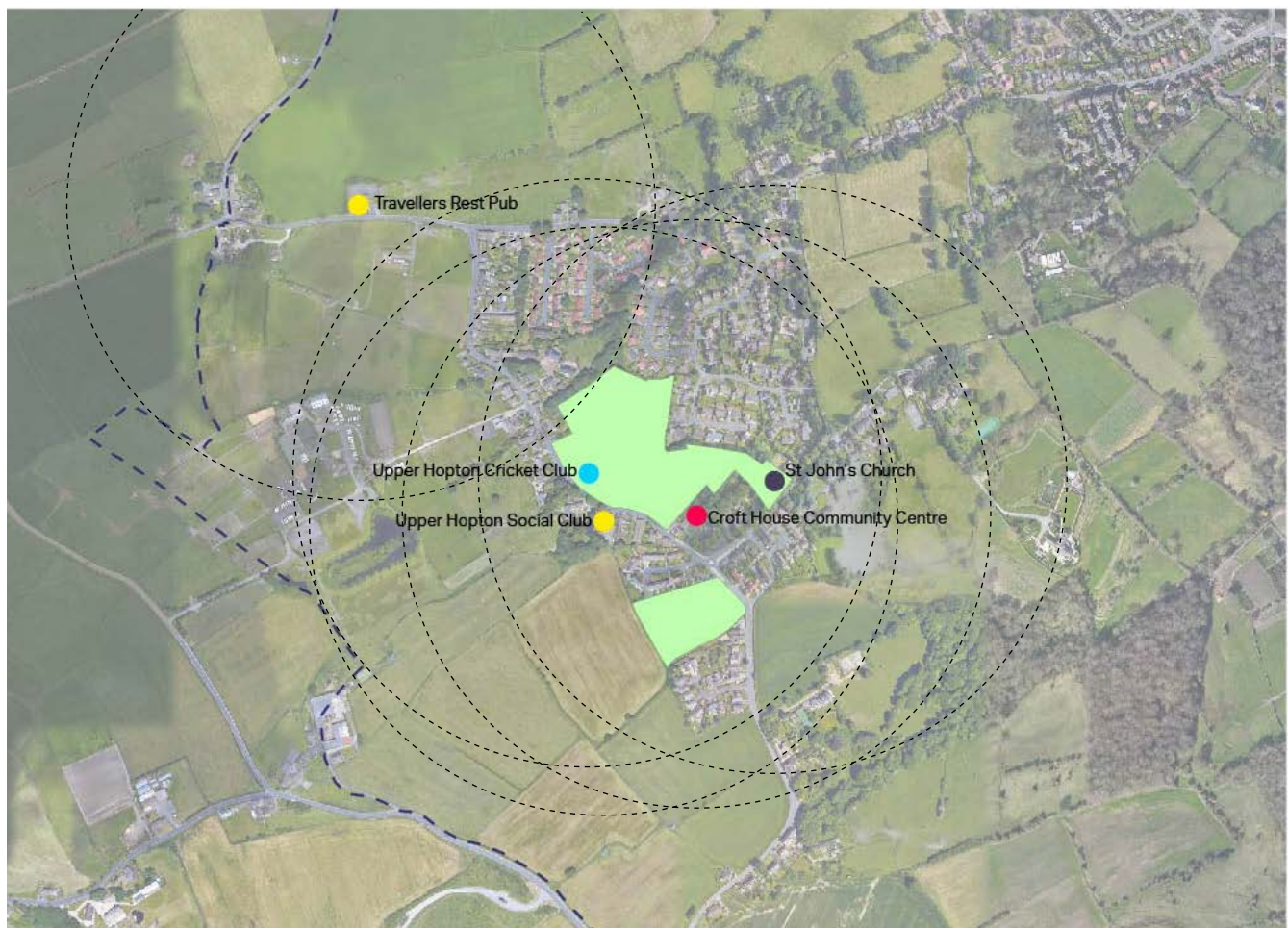


Figure 28: Village facilities with 400m walk bands

2.8.10 Village Facilities

Activity and facilities in the village are focussed around the higher, south-west edge and includes a variety of community facilities and institutions including; St. John's Church; Croft House Community Centre; Upper Hopton Social Club and Upper Hopton Cricket Club. These also occupy and surround key green spaces and open spaces including St. John's Churchyard and Sutcliffe Memorial Ground (recreation ground). The Travellers Rest pub sits to the north-west, overlooking the Calder valley to the North East.



2.8.9 Grain and Density

Historically a very low-density settlement with clusters of key buildings of short runs of terraces that create streaks of density within a semi-rural setting – the village has now changed and much of the new core is at a density typical of more modern hillside suburban housing (circa 25 DpH).



Figure 29: Figure ground sample

Indicative densities

- Indicative lower density houses: 15 DpH
- Indicative terraces density (small sample areas only): 35 DpH
- Indicative infill density: 25 DpH
- Average settlement density: 20 DpH



Figure 30: Hollin Hall cluster of higher density to form key gateway

Plot size, privacy and definition

Many of the older and larger homes are set back from the street behind substantial walls and this creates some very large private gardens that have been cultivated over time. In this way the public and private realm are very well defined. The overlooking of the public realm in this sense is somewhat less than other settlements but the nature of the boundary walls give presence and nature of ownership that make clear you are in someone's village and a wealthy one at that.



Figure 31: Figure ground sample

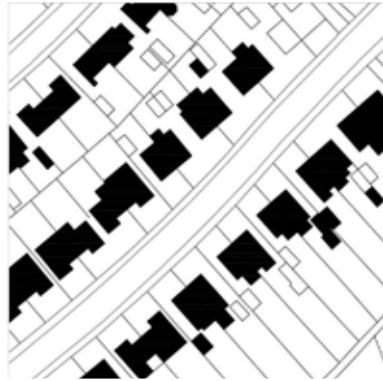


3. LOCAL DISTINCTIVENESS & CHARACTERISATION STUDY

Farmsteads and Hamlets



Suburban (lower density)



Town Centre (mixed-use)



Industrial / former industrial



Urban (higher density)



Village (semi-rural)



Figure 32: Figure ground comparison study

3.1 Settlement Character Overview

A valley bottom settlement in the Calder Valley, spanning the River Calder and expanding to the north side of the river up the shallower (but still well-inclined) slopes of the valley. The town has preserved many of its heritage buildings whilst continuing to expand and infill greatly over time. Several smaller settlements have combined to form the overall urban area of Mirfield today, including Towngate, Little London City, Green Side, Lee Green, Battyeford, Northorpe and

Eastthorpe Lane (now Huddersfield Road) to the north of the River Calder and Hopton Fold to the south. The latter half of C20 saw the growth of modern housing estates, infilling the land between historic settlement.

The township has one designated Conservation Area (the village of Upper Hopton) which sits within the Green Belt. However, within the main body of the town north of the Calder there is some good and historic character to preserve and 42 listed buildings and key historic sites that contribute to the overall distinctiveness, principally focussed on the historic settlements.

3.1.1 Urban - Rural Character Areas

A primary purpose of this Design Code is to help generate authentic and locally distinctive design responses to the existing settlement context and range of urban - rural situations across the parish and its range of topographies and landscape features within the setting of the Calder River Valley, setting the tone for regeneration and future development.

As such we have defined character areas that help understand the broader pattern of urban (and rural) settlement, to which some of the design codes and guidance can be applied.

These character areas identify areas with common characteristics be they, historic period, landscape character, topography, function and predominant built-form type within an area. The simple break down is; town centre; industrial; urban; suburban; village and rural.

3.1.2 Historic Focus Areas

A primary purpose of this Design Code is to set out an evidence base that helps to protect and enhance the historic character of the town and its various significant heritage assets within the setting of the Calder River Valley, setting the tone for regeneration and future development.

As such we have studied numerous Historic Focus Areas that help understand the historic origins and pattern of urban settlement, from which some of the design codes and guidance can be drawn in order that the historic underlying character of Mirfield can be supported and enhanced.

These focus areas were identified from historic mapping studies and consultation with the NP group who sense checked the selection to provide an accurate and representative sample across the parish,

Takeaway:

Response to the sloping topography (and floodplain); views and relationship to the valley and surrounding countryside; connecting front boundary walls and the predominance of a sandy coloured brick and stone as a building material are the overriding characteristics of the many pockets of historic Mirfield that survive and existing later development - this is something that new developments must understand and follow on these traditions or innovate with purpose.

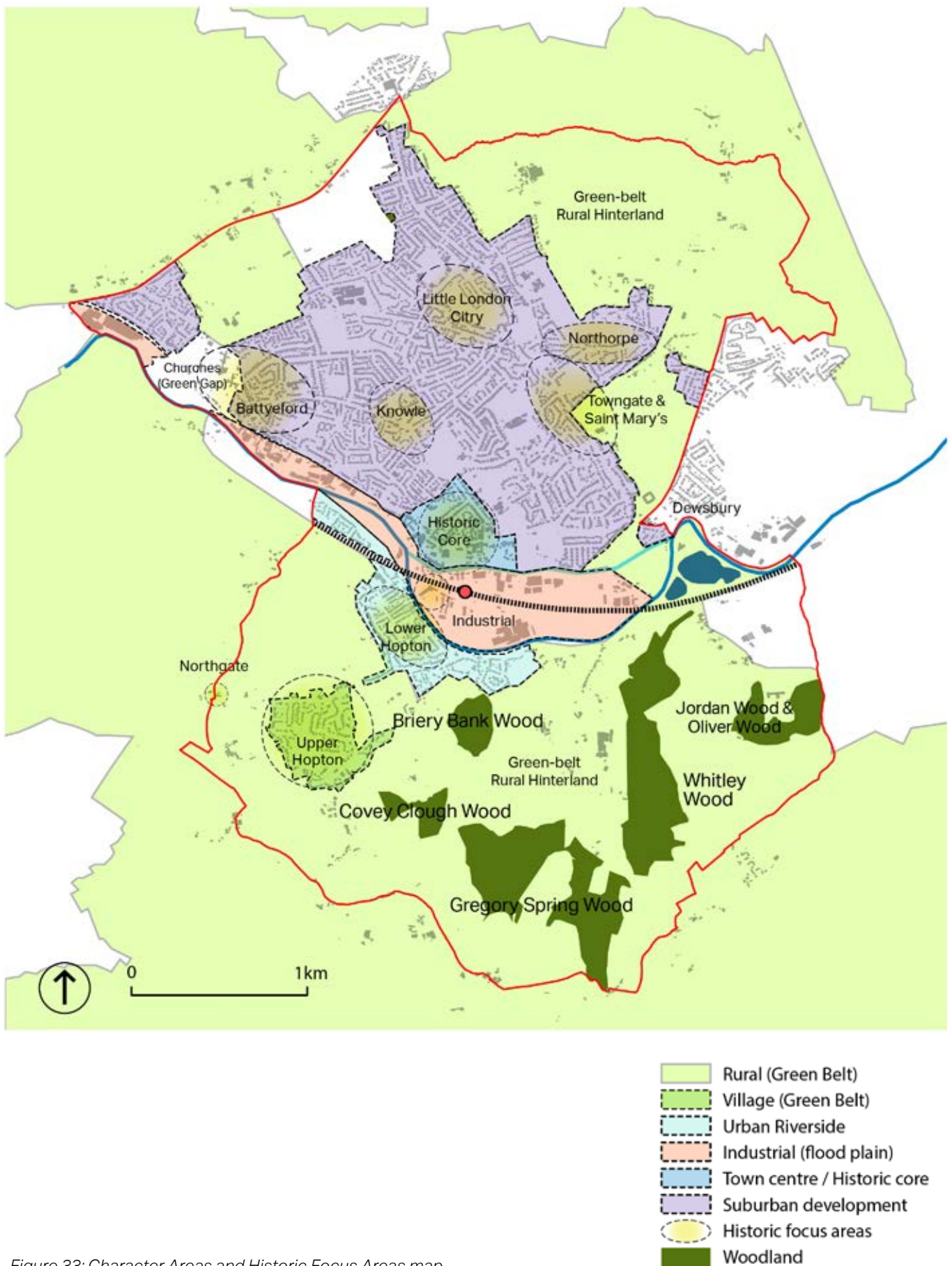


Figure 33: Character Areas and Historic Focus Areas map

3.2 Historic Focus Areas

Most areas mentioned originated as a small settlement, sub-urban area or hamlet, often based around farming and industry, now having merged over time to become a continuous or closely linked urban area (with the exception of Upper Hopton village within the Green Belt).



Focus Area 1: Upper Hopton Conservation Area

Settlement character overview: A semi-rural hillside village overlooking the Calder Valley adjacent to Lower Hopton/Mirfield. The village has preserved much of its historic character as well as continuing to evolve and grow over time. It remains distinctly separated from the main urban area of Mirfield.

A significant proportion of the main village streets (historic lanes) and trees/buildings that face them are protected in a Conservation Area. Therefore, the primary purpose of this Design Code is to protect and enhance this character within and around the setting of the Conservation Area.

Throughout the conservation area there are many good and historic qualities worth preserving and a scattering of listed buildings (5) across the village as well as protected trees and walls.

Characteristic features:

- Buff coloured stone / brick is the predominant building material across the village;
- Large detached properties set back behind walled gardens, also terraces
- Open spaces with long distance views over the Calder valley;
- Stone walls and hedgerows lining property boundaries;
- Triangle of historic lanes forming 'the face' of the village;
- Infill development within the core of the village (on former quarry site);
- Mature landscape setting and large trees lining streets;
- Halls, barns, farmsteads, villas, terraces and cottages provide variety; and
- Open spaces on higher ground to the south.



Architectural details:

- Brick /stone construction
- Quoin detail on grander buildings
- Stone slate roofs
- Coursed stone walls (1-2m in height)
- Countryside walls – lower dry stone
- Stone mullioned casement windows
- Stone jambs cills and lintels
- Semi circular stone surround windows
- Stone Mullions ((Hopton Hall)



Listed Buildings

Upper Hopton has 5 Grade II listed buildings:

- Main barn to Hall Farm
- Church of St. John
- Hopton Hall
- Numbers 124,126 and adjoining barn
- Northgate terraces (22-34)

Slate

Render

Stone



Figure 34: Typical material palette



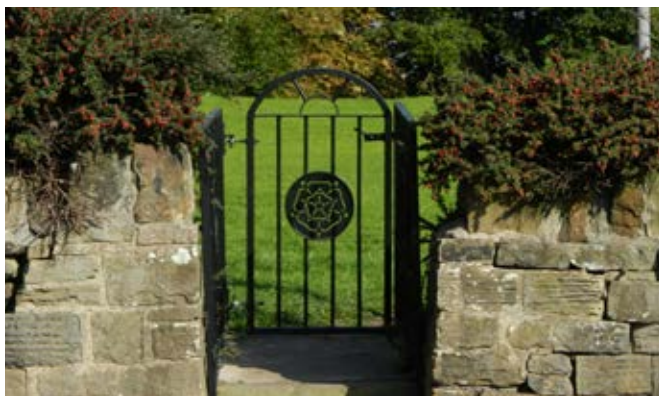
Roofscape photograph



Redevelopment with rusticated stone and stone mullions



Contrasting stone and render buildings



Public realm and landscaping photographs



Focus Area 2: Town Centre (Eastthorpe)



Huddersfield Road building detail

This attractive town centre is based around the Huddersfield Road (formerly Eastthorpe Lane) which effectively forms the High Street, linking into Knowl Road (more residential in nature), Station Road and Newgate both linking back to industrial areas across the Calder and Hebble Navigation. This relationship with industry (along the canal, rail and river) cemented Eastthorpe as the centre of Mirfield despite being pre-dated by Towngate.

Although not the most historic of Mirfield's settlements but based on its status as the overall centre it is the core of the settlement and has much historic richness and value.

Mirfield's wealth and prosperity from its heyday is exhibited in a variety of buildings along its 'high street' which have been well preserved and are often richly detailed (particularly moving further east) including; corbelling, decorative

window surrounds, keystone arches, quoins and date stones. The rough finish sandstone brick and slate roofs define this Yorkshire mill town aesthetic.

The main Town-Centre area is surrounded by streets containing Victorian terraced properties with imposing gables and sash windows. The ironwork, which has been retained on some boundary walls, matches the gates that add to the character of the area. Queen Street, Knowl Road and St. Paul's Road have some excellent examples, though there are more modern buildings interspersed amongst them. These include many of the detached properties. Knowl Road is an impressive street particularly.

To the east of the town centre are major open spaces of Mirfield Showground and the significant sports grounds that straddle the A644 and reach down to the canalside.

Characteristic features:

- Sandy brick and stone predominant;
- Town houses, terraces, mixed-use shops and offices;
- Grand town houses/club on Knowl Road;
- Low stone walls, stone piers and metal railings;
- Vistas and views to the countryside (southerly);
- Retaining walls and additional storeys taking up level changes (s. of high street);
- Precedent developments: Mixed-use Apartment conversion (Speights Lighting), St. Paul's Lock, Beech Grove (suburban);

Negative factors:

Modern developments between Eastthorpe Lane and the Canal have somewhat fragmented the town centre, notably the Co-op supermarket and its open surface car park create a large gap in the urban form that is not in keeping with the scale and enclosure of streets and spaces in the town centre. The fire station is another example of an unfitting architectural response capturing the view at the end of Knowl Road.

- Gaudy / oversized business signage; uncharacteristic infill development or extensions; street clutter and road signage; erosion of the public realm character, highways and carriageway dominance (guard rails); UPVC windows and doors/ modern shopfronts.



Boarded-up buildings detract from the environment and require bringing back into use, white painted railings and painted lines on the streetscape detract further overall



This well built and solid bus stop is pleasantly fitting with the town centre aesthetic



Glimpsed view down a side street over the Calder valley to hills



Huddersfield Road: This mixed-use street is a key link to a neighbouring settlements and its slight elvation from the valley bottom and views out creates an interesting urban high street with visual connections to the landscape. The plot sizes are generally narrow but include larger individual dwellings on the periphery. It is book-ended by a park (west) and show ground (east).



Architectural features:

- Regular coursed ashlar / rustication brick
- Quoins; corbels, Stone jambs, sills and lintels
- Stone slate roofs; Chimneys, Close cornices
- Coursed stone walls (1-2m in height)
- Stone casement windows; Mullions
- Key- stone window & doorway arches

Three-storey building with active use on ground floor



Grand wide-fronted pair of buildings on Queen Street



Ings Grove Park provides relief and an important green recreation space within the urban town centre



...The same cannot be said for the fire station at the end of Knowl Road, a necessary facility no doubt but not helping the townscape or attractiveness of a fine town centre

Listed Buildings:

- Calder and Hebble Navigation Flood Lock at Newgate Bridge Grade II
- Former Lock-Keepers Cottage Grade II
- Church of St Paul Grade II
- Trinity Methodist Church Grade II
- Water Hall Grade II
- Front Wall, Railings and Gatepiers to Trinity Methodist Church Grade II
- Gatepiers and Gates to Water Hall Grade II
- Mirfield war memorial Grade II
- Detached House Approximately 50 Yards South East Of Number 115 Grade II



Trinity Church terminates the view at the end of Fenton Street

Focus Area 3: Original Settlement & Surrounds (St. Mary's and Towngate)

St. Mary's Church at Towngate is the oldest known church in the township of Mirfield and likely the most historic focus of the whole settlement

Towngate in the middle of the 19th century was a linear development running for around 1/3 of a km on the northwest-southeast route of Towngate lane. Cottages were located on both sides. The Old Rectory is from C16 (former rectory from 1300).

There are four listed buildings in the Towngate locality including the church and rectory (Grade II*). There are two other Grade II Listed - Ivy Lodge at the northern end of the village on Crowlees Road (with some 17th century stonework) and Over Hall (1721) to the north of the village.

Much of the Towngate area has been consumed with suburban redevelopment.



Hedgerows provide privacy to this cottage



Stone walls and mature trees on Chruch Lane gives pleasant enclosed green character that contrasts with denser areas



The Old Rectory



Ivy Lodge (barn)



New homes at Church meadow use sympathetic materials and some buildings are set at 90° to the street



Wellhouse Court Mews is a modern take on a mews court typology with sympathetic detailing such as corbelling, stone window surrounds and rusticated stone

Characteristic features:

- Sandstone worker cottages (wide) & narrow terraces on Towngate
- Grander town houses on Flash Lane / Wellhouse Lane
- Stone walls and hedgerows, gateposts and railings
- Some historic lanes (e.g. Towngate and Pinfold Lane)
- Chimneys, corbels, stone lintels and sills, slate roofs
- Stone walls taking up level changes and mature landscape on Church Lane
- Ecclesiastical buildings and institutions
- Lanes and views aligned to St. Mary's

Precedent developments: Church Lane, Vicarage Meadows, Wellhouse Lane Mews Court



Front parking erodes boundary treatments, changing the character of the street negatively the more often it occurs. The practicality of parking on some streets is understood but this should require permission in historic focus areas.

Cottage set at right-angles to the street*St Mary's Church**Mirfield in Bloom display**Pinfold Lane*

Listed buildings:

- The Old Rectory Grade II*
- Ivy Lodge Grade II
- Thorpe Cottage Grade II
- Stocks 20 Yards To South Of Main Gate To Church Of St Mary
- Boundary Wall, Railings, Gates and Gate Piers To South and West Sides Of Church Of St Mary
- Raised Grave Slab To Wraith Family, 15 Yards To South West Of Tower of Old Church Of St Mary Grade II
- Tower Of Old Church Of St Mary Grade II
- Tablet Fixed To Tower Of Old Church Of St Mary Grade II
- Church Of St Mary Grade II*

Focus Area 4: Little London City / Lee Green / Green Side

Remnant buildings of these three part-detached hamlets of GreenSide, Little London City and Lee Green, principally along Lee Green and Greenside Road, are interspersed with later infill development from Greenside Road to Old Bank Road. Settlement in this area appears to have been early Industrial Period with short rows of cottages, houses and at least one chapel. Wellhouse Farm is a timber framed house dating to 1576. The land in the triangle between these lanes is now Greenside Estate recreation ground.

The modern focus of Greenside is the local centre with several local shops and takeaways. There is a greater instance of red brick buildings in Greenside (further north and east), and Old Bank Road as far down as Water Royd Lane. Old Bank Road at the end of Lee Green is also a local centre.

Characteristic features:

- Stone walls at front property boundaries and surrounding larger plots/ open spaces;
- Sandstone terraces, houses line lanes and set close to the pavement, or at 90°
- Open spaces to the east giving a semi-rural feel along Greenside Road;
- Historic unsurfaced lanes behind Lee Green such as Savile Place/Court;
- Infill developments and courts in the backlands of the main lanes; and
- Modern suburban red brick development towards the northern tip of the parish.



Historic mapping extract



Single storey extension to building on Lee Green is subservient to the house and the detailing at the roof junction/eaves is good



Development at Greenside Close has sympathetic materials, well proportioned windows and is of a comparable scale



Converted three-storey building on Lee Green, cleaned and restored



Figure 36: Figure Ground sample

◁ *Figure 37: Birds-eye aerial view*



Sandstone terraces / buildings are predominant along Greenside Road, and together with the stone walls defining plot boundaries, giving it a strong semi-rural character.



Single storey stone cottage conversion at junction with the Reynards



Stone terraces with end gable

Focus Areas 5: The Knowle

A concentration of worker housing and associated settlement including grander villas at Knowle from the later half of C19 remains around the triangular arrangement of Knowl Road, The Knowl and Back Knowl Road. There appears to have been at least one chapel and it is thought that a green could have formed the triangle at the centre of these lanes originally. This is interspersed with later infill development of a suburban nature. A local centre remains here with a series of local shops and also a nursery in the former school building.

Characteristic features:

- Sandstone narrow terraces and larger sandstone villas;
- Back of pavement, small front gardens, or larger front gardens for villas
- Sandstone walls defining plot boundaries and the public realm, giving it a strong public and private definition with a good sense of enclosure;
- Institutional / civic buildings, eg. chapel/ school adapted for modern use (e.g. nursery)
- Adaptable terraces/ townhouses that allow shopfronts to replace ground level;
- Traditional buildings that 'turn the corner' at junctions;
- Mature hedgerows and planting on Knowl Road; and
- Typically 1 – 2.5 storey buildings.



Knowl Road



St. Peter's Close - This inward facing development for the elderly, with its front doors on this cul-de-sac has an unclear relationship of building to street (sawtooth) and lacks definition in public and private space. It would benefit from the simple introduction of low boundary walls to better define public and private space and link into the street in more ways than it does. This would give a greater security to residents also.



Figure 38: Figure Ground sample

◀ Figure 39: Birds-eye aerial view



Stone boundary walls, Knowl Road



Larger detached properties, Knowl Road



Figure 40: Shillbank Lane terraces

Focus Areas Study 6: Northorpe

The coming together of Northorpe Lane, Crossley Lane and Shillbank Lane forms the modern hub for this area based around the Plough Pub and Shillbank Stores.

This small village (formerly Hamlet) is approximately 800m northeast of Towngate. Northorpe was also a linear development, with the lane widening in the centre to form a small irregular green by the hall which corresponds with Northorpe Lane today. Three buildings in the village are Listed. These comprise Northorpe Hall of 17th century date, the hall's aisled barn also of 17th century date and a house dated to 1701.

Sandstone terraces are found along Shillbank Lane and on Crossley Lane where the terrace is at 90° to the street, aligned with the contours of Crossley Hill rather than stepping up it which is a typical arrangement found in Mirfield.

Characteristic features:

- Views across the Calder Valley on southern aligned streets
- Victorian era light sandstone brick terraces (worker housing for mills etc.)
- Narrow fronted sandstone terraces with pitches roofs, chimneys and simple, well-proportioned windows. Occasional wide fronted terraces on Shillbank Lane also;
- Low boundary walls and short setbacks to properties on main streets (e.g. Shillbank Lane) and hedgerows for privacy in front gardens or sometimes railings atop low walls;
- Terraces set to back of pavement on steeper streets, sometimes with a verge separating the pavement from the street, or lane access for terraces aligned with the contours;
- Open spaces, estates and semi-rural lanes are defined by stone / drystone walls;
- More introduction of red-brick from Edwardian period properties and later;



Figure 42: Northorpe Hall boundary wall



Figure 43: Shillbank Lane

The infill areas of Northorpe are mostly consumed with unremarkable suburban development with red brick estates more common in this area that contrast with the predominant character of the sandstone terraces on the main streets of the area.

Removal of front boundary walls for parking on traditional terraces properties is understandable from a liveability perspective but erodes traditional character overall (see above).



Figure 45: Birds-eye aerial view



Figure 44: Shillbank Lane



Figure 41: Figure Ground sample

Figure 49: Shillbank Lane



Figure 50: Northorpe Lane



Figure 48: Shillbank Lane



Figure 47: Flash Lane



Figure 51: Figure Ground sample

Figure 52: Former Three Nuns pub at the former Kiklees Priory

Focus Areas Study 7: Battysford

Key Opportunities to consider:

1. Public realm opportunity area

(Huddersfield Road) – there is potential to improve the hard landscape and introduce planting and traffic calming to reduce vehicle speeds and create a more comfortable pedestrian environment (and allow traffic to flow). E.g. consider wider pavements; planting between on street parking; higher quality finishes at key junctions to enable level-crossing for pedestrians and wheelchairs.

2. Public square opportunity area

(Tesco) – the setting to this important 'header building' (terminating the vista from Station Road) is eroded due to the piecemeal and irregular hard-landscape design, particularly clutter including signage; lighting bollards and bins which could be consolidated. The disabled parking bays could be reprovided discreetly on a higher quality hard-landscape surface (with flush block paving rather than tarmac and white lines which degrade the setting and highlight car dominance).



Battyeford area:

- Church Of The College Of The Resurrection Grade II
- Over Hall Grade II



Figure 53: Huddersfield Road

◁ Figure 54: Birds-eye aerial view



Figure 55: Clockwise from top left - Church of the Resurrection; Copin Hall Lane; Kitson Hill Road and Half Penny Bridge (Battyeford Toll Bridge)

Focus Area 8: Lower Hopton

Lower Hopton sits on the lower slopes up from the southern bank of the River Calder. Calder Road / Granny Lane follow the river, forming the main frontage. Together with Hopton Lane, which leads up the slope to Upper Hopton, they form the main streets.

The Flowerpot Inn, Hopton New Chapel and Ledgard Bridge create a series of landmarks along Calder Road. Lower Hopton as a place name arrived with the provision of a series of light sandstone brick terraces, one along the River Calder by the Flowerpot Inn (Philip Royd Terrace) and the remainder perpendicular to the Calder between the Chapel and Ledgard bridge. Much of this street structure and the terraces survive and the ends of these terrace blocks facing the Calder are established as local shops, pubs and services, forming a local centre for the area.

Characteristic features:

- Views to the River Calder, its bridges (Ledgard Bridge / Hopton New Road)
- Ledgard Wharf (redeveloped) is a local landmark, across the River Calder
- The Chapel, set back from Calder road, is a grander institutional building and local landmark
- Stone wall with crenulations atop running the length of the River Calder
- Victorian era light sandstone brick terraces (worker housing for mills)
- A highly connected built-form – terraces stepping up the hill



Typical sandstone terraces stepping up the slope



View to Ledgard Wharf from Calder Street (across river)



Calder Road (River Calder to left behind stone wall)



Hopton New Chapel

Listed Buildings:

Hopton Congregational Church Grade II*



Calder Road shops

Architectural features:

- Rustication brick, coursed ashlar stone
- Quoins; corbels, Stone sills and lintels
- Stone slate roofs; Chimneys, Close cornices
- Coursed stone walls (1-2m in height)
- Vertical proportioned windows for good natural light
- None or recessed canopies / porches
- Doorsteps / stepped buildings
- Traditional shopfronts



Later homes in Lower Hopton of a different scale, with different proportion windows and openings to the traditional terraces



Contemporary designed homes on Hopton Lane use sandstone and decent sized windows for natural light



Hopton Bottom

3. Potential to develop frontage (Co-op car park, Huddersfield Road edge)

– the lack of building along this edge and views over an expansive car park lets down the quality of the townscape with some poor views over surface car parking and the single-storey Co-op building (despite the lovely valley backdrop). Glimpsed views to the valley and car park could be retained to maintain legibility but the overall frontage on this outlook would help to improve the high street if it could be redeveloped in a more considered fashion. The Co-op building is an example of the type of box-retail building surrounded by car-parking that should not be allowed in a largely traditional and human-scale town centre. There are ways to accommodate this however such as 'wrapping' the frontage.

4. Green link to Station - There should be a clear strategy to make a green and pleasant route to the station, if space is tight some tree planting within this street corridor could create legibility and attract active frontage to oversee this key link within and to the south of the town centre.

Focus Area 9: Calder Valley Industry / Floodplain



Figure 59: Figure Ground sample

◁ Figure 60: Bridge

Mirfield grew up as an industrial development based around textiles, but later this industrial emphasis shifted towards coal and malting. There have been as many as thirteen malt houses in Mirfield. Industry has changed to include more modern, highly polluting industry adjacent to the river corridor which is a sensitive environmental receptor so cleanliness must continue.



Public art under the railway bridge- graffiti wall welcoming visitors to Mirfield helps orientate visitors and brightens up the industrial setting around the station

Listed buildings:

- Sheep Ings Farmhouse and Attached Barn Grade II
- Boat House Grade II
- Broad Oakes Grade II
- Cottage to Rear of Broad Oaks Grade II
- Walls to East and South of Broad Oaks Grade II
- Railway viaduct MVN2/196, Wheatley's Bridge Grade II
- Railway underbridge MVN2/194, Hirst Lane
- Ledgard Bridge (over River Calder) Grade II
- Railway Bridge over River Calder (Mirfield Cooper Bridge Line) Grade II
- Calder And Hebble Navigation Cottage At Shepley Bridge Locks Grade II
- Calder And Hebble Navigation Double Lock At Shepley Bridge Grade II



Figure 61: Public art

Figure 62: Birds-eye aerial view



Figure 63: Clockwise from top-left - Canal bridge; new canalside homes; South Brook Gardens; and Ledgard Bridge

The scale and massing of buildings in this area, stemming from the Victorian industrial era, is generally greater and the location on the floodplain means that they are often visible along the valley from a wide variety of locations by river and canalside.

The Calder Valley acts as a “green corridor”, due to its many linear features and extensive tracts of water. The presence of a navigable waterway and a history of boat building in Mirfield is an important asset to the landscape, although unfortunately, much of this is hidden from view.

Orientation and legibility are key issues upon arrival at Mirfield station and choosing which direction to head in. The general environment is lower quality and with much industrial use visible. Great efforts have been made to improve the street-tunnel under the railway with lighting and murals that animate and brighten it.



The scale and massing of buildings in this area, stemming from the Victorian industrial era, is generally greater and the location on the floodplain means that they are often visible along the valley from a wide variety of locations by river and canalside



This combination front parking, bins and low quality boundary creates an unwelcoming and unattractive street scene that is unbefitting of what is otherwise a well considered townhouse development on Back Station Road next to Ledgard Wharf



The extension to Ledgard Wharf is fittingly designed to relate to the proportions and materials of the original building but yet clearly expresses that it is a later addition through the glazed core that divides new and old.

Focus Area 10: Rural / Farmsteads & Edges

Stocksbank Road and Nab Lane form the spine of Battleyford which is a neighbourhood with its own local shops, schools, pubs and churches, somewhat dispersed throughout.

Much of the historic urban fabric lies between Huddersfield Road and Stocksbank Road.

The land rises to the north and there are views down north-south oriented streets to and over the Calder Valley. Development further north

The area is a mix of older stone terraces and houses on key streets (often the connected streets such as Stocksbank Bank Road, Bank Street, Coppin Hall Lane) and on narrow lanes such as Bracken Hill. Modern suburban cul-de-sac or loop developments sit in-between. There is a complex of institutional buildings associated with the church (Christ the King, Mirfield Monastery and Community of the Resurrection) which form a break in the urban form to the west and a care home to the north which occupies the site of a former estate with its stone boundary walls and parkland (sitting within the Green belt),

Hand Bank Lane and the Waste Lane are old lanes that have survived as later suburban development has filled in the fields between the Chapel and Hopton Lane and risen up the slopes above the original Lower Hopton terraces. Many of these detached and semi-detached homes use light sandstone brick to maintain some connection to the



Typical coutry lanes with stone walls and fencing that have a practical roll to prevent livestock escaping and define property boundaries



A typical complex of farmhouse and related buildings that form courtyards and sit within a variety of plating including a stand of trees which can help to settle visually or minimise strong winds

original character of the area but the layout and feel is simply very different due to the levels of enclosure, building heights and proportions.



4. DESIGN GUIDANCE AND CODES

4.1 Overarching Design Guidance

The design codes and guidance set out in this section will relate to major development sites, infill development sites and redevelopment of existing buildings throughout the Mirfield area.

The design guidance below is focussed on overarching responses and directing the key themes of character, sustainability and community consultation that should impact on the quality of all local design proposals and help to deliver and preserve high-quality buildings and places.

4.2 Protecting historic assets and heritage as a catalyst for regeneration

General Approach

The approach to heritage should be one of protecting historic assets, both designated and undesignated, but understanding that the urban fabric is complex and interwoven. As such we cannot simply put a boundary around heritage assets and say 'hands off' and indeed the urban areas of Mirfield today are testament to the fact that we can develop and grow with our heritage as part of our everyday neighbourhoods. Many historic estates have been built upon, however, some less successfully than others. Also, built heritage is perhaps a common focus but the landscape has a heritage as well, as does the

cultural landscape and it is important to relate back to these too. Also, archaeological finds, by definition, remain unknown until found and so is important to look out for these on sites.

Therefore, understanding what remains and is of value and then preserving it, particularly the highest value areas such as statutory designations, listed buildings and conservation areas is a key aim. Another aim is to enhance the heritage that we have with new development, particularly improving on the poor developments of the past which through redevelopment can significantly improve the setting of heritage assets that have been tarnished by insensitive, poorly designed schemes. Heritage may also be a lynchpin for new development, giving it meaning and helping ensure that it is of its place. Finding new uses for historic buildings such as has happened at Ledgard Wharf can connect the Mirfield of today with the Mirfield of old, continuing the story of this historic asset and making use of the existing fabric.

Heritage that is of national significance is important but so is heritage that is locally significant and this ties back to the 'cultural landscape' mentioned and the embodied memories that significant places build up for many people over time. This mental image of the town that every resident will have is their own heritage and important to understand for different groups. The town centre is one of the most active and adaptable, changing spaces but also has a high concentration of Mirfield's heritage and this shows us that heritage is dynamic and integral to attracting visitors, commerce and also fundamental to the identity of this Yorkshire mill town and its residents, and their closely related identities.

4.3 Early Engagement & Community Consultation

Consultation with different demographics and stakeholders with regular communication and close liaison with community groups must form a key part of the design process from an early stage (where designs can be affected and improved). This must continue throughout Outline Application, Reserved Matters stages as part of both design process and formal consultation.

Following up with monitoring and surveys of built development can also help with innovation towards delivering sustainable, well-loved developments. Using the relevant design code tables in the following section is a useful way to structure dialogue with stakeholders and evidence local distinctiveness within designs, as well as appropriate precedent designs.

4.4 Enhancing Local Character & Distinctiveness

4.4.1 Preserve and Enhance Local Character Features

Development must be in keeping with locally distinctive features, including; predominant building materials & colour palette, stone and brick boundary walls, and original architectural details such as; quoins, corbels, dentils, fenestration and porches;

These local character features must be preserved and enhanced where possible within all of the character areas; and

Respond to character areas and take inspiration from distinct Focus Areas.

4.4.2 Justifying Alternatives to The Codes and Guidance

Exceptions to the code are possible but must be robustly justified and in keeping with the spirit of The Design Code. The code is not intended to stifle creativity or excellent design solutions that are in keeping with or complimentary to the historic character of the town or enhance the sustainability or meet the local needs for housing within the town. These should be considered on their merits but must be both high-quality and well-designed. Copying the past in a pastiche way with 'bolt-on' elements is not a suitable design response. Recreating authentic details and features on the historic buildings in town is allowed if it respects the craftsmanship of the original local features. Likewise, reinterpreting the local vernacular architecture in an up to date way, using up to date building technology and materials is allowed but requires excellent design proposals to enhance the character of the historic townscape and to help meet sustainability objectives.

4.4.3 Listed Buildings:

Any application for listed building consent for the alteration, extension or change of use of a listed building, and any application for planning permission which would affect its setting should pay special attention to the desirability of preserving the listed building or its setting or any features of special architectural or historic interest which it possesses.

Demolition of listed buildings will not normally be considered acceptable unless it can be demonstrated that the building has no beneficial use and no potential viable use and the structure of the building cannot be made sound.

4.4.4 Response to Local Building and Landscape Character

Developers and their designers must set out a clear response to the character area/s in which their development is sited or adjacent to (see character plan), including landscape character considerations if relevant;

Designers are not required to mimic the existing design period of an identified character area in the form of pastiche, though this is not ruled out if authentic;

The character response should be clearly outlined according to all categories of reserved matters detail design including; Means of access, Layout, Scale, Appearance and Landscaping.

Designers should respond to the character area with one of the following 3 approaches considered in the order set out (1-2-3);

- 1. REFLECT CHARACTER** – Development must clearly respond to and take on existing characteristics found within the character area, street or site across most design aspects including; Scale, Form, setback and materials in order to fit in and reflect the existing character.

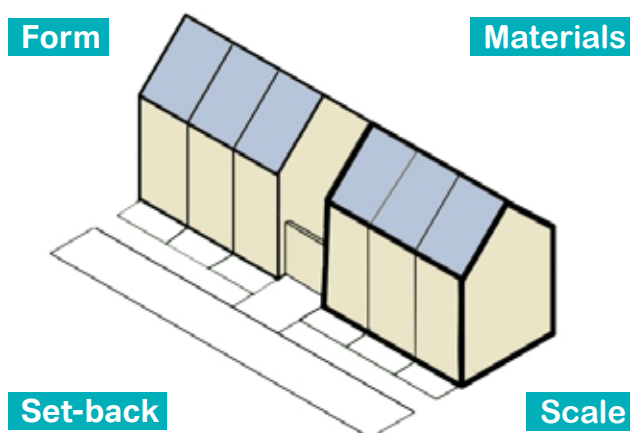


Figure 64: Sketch of new terrace extending from and harmonising with existing terraced form on a street

When to use:

When there is a strong, consistent, good character in the area or street that works well and it would be preferable to reflect the character in new building due to the response to townscape and landscape.

Conservation Areas will require strong adherence to the existing character as a starting point, without stifling innovation.

Example: In an area such as Lower Hopton a new terrace provided on a street of terraced houses that matches the scale & massing (both the plot and building) will complete the existing layout and character of the street. If combined with local materials and details (although these could be modern) then the building will reinforce the local context.

This is not to say that in key locations a detached house might hold a view or provide relief, according urban layout principles of townscape which the Victorians employed.



Figure 65: Terraced housing type photograph

2. COMPLEMENT CHARACTER - If the local context permits (e.g. existing variety) development may do some things slightly differently (e.g. introducing visually compatible and complementary characteristics that fit within the overall street or development area and therefore adds to the overall character and quality in a way that adds character (e.g. matching in design aspects of form, setback and scale but also introducing additional materials or formal elements).

When to use:

When there is a mixed character in the street or area, no predominant style or largely unexceptional character. Here it would be apt to maintain the character of the street and plot series but there may be more scope to vary the buildings appearance by choosing materials and details that **complement** those existing or introduce new high quality design aspects



Example: A new semi-detached house on a street of semi-detached houses that broadly matches the overall scale and setback of dwellings on a suburban street but utilises high-quality materials and detailing that are contemporary in style and add something positive to the character area. Traditional boundary details such as a low masonry wall becomes instrumental in linking the architecture into the streetscene in a joined up fashion despite its differences. It should be noted that many existing buildings change (e.g. render applied, contemporary window replacements so there is often a degree of variety existing in modern suburban homes).

This positively contribute to the overall appearance of the street scene. The materials used are unable to match all the different buildings on the street but their good quality is evident and creates a positive addition to the existing mix.



Figure 66: New houses on Towngate (Mirfield) and Covey Clough Court (Upper Hopton) introduce a varied but attractive and complementary palette of materials and detailing. In the former case this is strictly contemporary, whereas around the conservation area motifs and detailing may be reinterpreted but are derived from historic detailing found in the area.

3. DISTINCTIVE STYLE – boldly doing something that is markedly different in character requires exceptional design quality and innovation. Still, it must not detract from the existing character of the area and must fit locally to create a coherent street-scape and add to local distinctiveness within the townscape.

When to use:

When there is an eclectic building character presented to the street or buildings are set further back on plot and much less visible due to boundary walls or landscaping so they co-exist easily.



Example: A detached house on a street of large plots houses that matches the layout (position on plot and set-back behind boundary wall) but **contrasts** in appearance by using a thoroughly modern palette of materials and details (e.g. render and large glazed areas). The house may forego the traditional chimney and instead exhibit a heat exchange flu that still is a nod to the vernacular (also justified for exemplar sustainability), the form may be a 'villa' (detached single dwelling) type like others on the street but there is more scope to vary the detailed form, e.g. footprint, height and roof-form due to plot size, landscaping and distance to other buildings. Individuality and self expression are what many people seek in a house and it is a balancing act to not deny this, whilst seeking to preserve townscape and traditional settlement characteristics that contribute to the overall attractiveness of the town. Innovation with purpose, high-quality design and designing with meaning are all reasons to depart from tradition. If these justifications are not there then this approach could lead to the degradation of local character.



Figure 67: Example detached houses (or villas) have larger plots to call their own and more space for landscaping to soften different styles of architecture. More conformity is still likely to be required in conservation areas, whereas a more privately situated house has more scope for contrast and innovation as shown by this architect designed 1960's style bungalow in Mirfield. Still, in Mirfield it is rare for buildings to completely depart from the common stone colour/ material.

4.5 Sustainability & Local Resources

The overall approach to sustainability should be not to treat it as a separate subject to design but to integrate it into the design process at all stages from location; use; layout and details.

4.5.1 The Dimensions of Sustainability:

The topics are far reaching and in summary may include the following according to the type and scale of development:

- Land (land use, density)
- Energy, renewables and carbon emissions (efficiency)
- Water (surface water drainage, flood risk, supply)
- Air quality and noise
- Biodiversity and landscape
- Heritage and culture
- Amenity, recreation and public space open space
- Transport
- Built form and public realm
- Employment
- Goods and services
- Housing
- Community, institutions and governance
- Education and skills
- Health

- Construction (sustainable sources and construction waste)
- Waste (reuse/repurposing/recycling)

4.5.2 Objectives

The overall use and amount of development will determine the range and level of these dimensions that are relevant. The primary objectives for the relevant dimensions are simple and common to all, whether water or energy:

- Make best use of and sustain local resources (e.g. using brownfield land);
- Reduce demand or build local capacity (e.g. Transport);
- Maximise efficiency (e.g. Waste);
- Reduce dependence on external resources (e.g. Energy);
- Increase multi-functionality, reuse and recycling of outputs (e.g. Water)

4.5.3 Scale of Design Proposals

The scale of proposals will vastly affect what can be achieved and so for some resources 'district' or neighbourhood solutions may give better overall results than 'micro' solutions.

The different scales that should be considered include:

- Whole Settlement
- Area Masterplan
- Single Building

These levels highlight that resources and inputs vary in their appropriate 'reach' or catchment. Most proposals will likely be at the level of a single building or small infill development. These will have a limited scope to address wide ranging approaches such as district heating, however, a major application of mixed uses including large space heating should investigate these.

Most individual buildings will look at small scale building-level solutions, e.g. thermally efficient building materials to reduce energy demand balanced with natural daylighting and solar-panels on roofs and water butts to maximise collection and use of sunlight and rain respectively.

Redevelopment of individual homes and small developments must still look at their contribution to overall settlement pattern sustainability and effect on use of resources as a whole which will shift with patterns of uptake of sustainable solutions amongst numerous existing buildings.

4.5.4 Local Resources

All developments should provide a summary account of locally available resources that are relevant to its scale. If a large enough development then this broad range of information will be gathered as part of the EIA, design appraisal and specific assessments of using local resources.

There are three broad types of resource:

- **Fixed or site specific assets (physical or biological)** – For fixed assets the overall aim should be to manage demand, promote multi-functional and efficient use and improve access;
- **Local services and activities (e.g. waste collection)** - The aim for services should be to build local capacity, increase efficiency and effectiveness and improve access; and
- **Consumables (e.g. electricity)** – The overall aim in terms of end use for consumables is to reduce demand by reducing the perceived need, reducing the actual use, increasing efficiency/ multi-functional use or all three.



Figure 68: Asymmetrical roofs with photo-voltaics aligned for solar orientation in these 'solar terraces'



Figure 69: Cladding and roof angle varied to provide visual interest and aligned to north - south orientation

4.6 Building types

Within the parish the characteristic building style often varies between each character area. For example, villas are characteristic in Upper Hopton and simple worker cottages are characteristic in Lower Hopton. A strong understanding of existing characteristic buildings, which contribute to the local character within the landscape character type where the new development is located, is essential to ensure that the style of new development is appropriate. For example, a new building inspired by the villas in Upper Hopton would be less appropriate in the Industrial valley bottom character area.



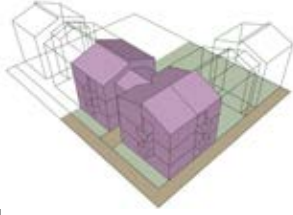
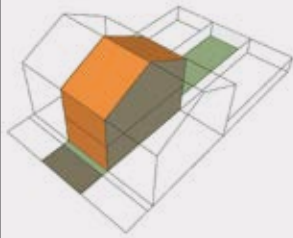
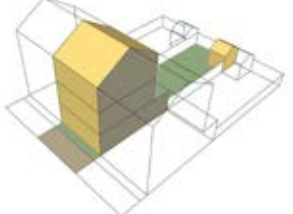
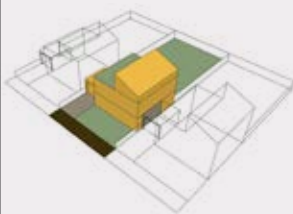
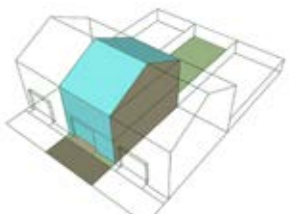
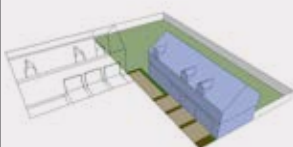
		Height	Max. width		Depth	Model	Application
Building Typology		Up to	Single	Combi.	Up to		Area
APARTMENT	<p>✓ - Mill / other conversion; Courtyard; Block; Linked-blocks; Terraced (vertical separation) & Mixed-use.</p> <p>✗ - Not tower or podium</p>	4 st.	10m	30m	21m		<ul style="list-style-type: none"> Urban centre; Urban riverside; Valley bottom; Main Streets
TERRACE	<p>✓ - Wide- front or narrow-front terrace</p> <p>✗ - Not more than 6 in a row</p>	2 st.	6m	30m	11m		<ul style="list-style-type: none"> Urban centre; Urban riverside; Valley bottom; Main streets; Suburban; Villages
TOWNHOUSE	<p>✓ - Wide-front or narrow-front Townhouse;</p> <p>✗ - Townhouse</p>	3 st.	6.5m	26m	15m		<ul style="list-style-type: none"> Urban centre; Urban riverside; Valley bottom; Main streets; Villages
VILLA (DETACHED)	<p>✓ - Detached; cottage; manor house;</p>	2.5	10m	n/a	11m		<ul style="list-style-type: none"> Suburban; Villages; Semi-rural, Main Streets; (farm/hill-top)
MEWS-HOUSE	<p>✓ -Mews; cottage</p>	2.5	5m	20m	9m		<ul style="list-style-type: none"> Urban riverside; (Minor Streets); Villages; Semi-rural
OTHER	<p>✓ - Carriagehouse; Flats Over Garages (F.O.G's); Bungalow; Barn; Out-house; Self-Build</p>	1.5	5m	15m	9m		<ul style="list-style-type: none"> Suburban; Villages; (Lanes) Semi-rural; (Courtyards)

Figure 70: Table of Residential building types table

4.7 Local Distinctiveness

Have regard to local building character. For example, where there is a history of designing and building with an individual character in an area (e.g. Upper Hopton) it is likely to be appropriate to continue this tradition.

New development should have an appropriate identity that makes a positive contribution to local distinctiveness, relevant to its urban character area. Innovative designs can integrate the needs of sustainability, energy use and respect for landscape character.


Explore the potential for new buildings to creatively reflect features on locally characteristic buildings within the relevant urban character type. This might include exterior chimneys, steeply pitched roofs, ornamentation or a mix of materials where they are characteristic locally.

New development should respect the scale, mass and form of the existing locally characteristic buildings within the character area where the development is located to avoid eroding local identity.

Non-local architecture, details and materials are common-place across the country. As such imported ideas cannot be forbidden entirely but where proposed should have a story behind them that adds to local interest.



Figure 71: Table of local distinctiveness traits by character area

		LOCAL DISTINCTIVENESS			
		Identity / Rootedness	Settlement Design	Development Opportunity Type	Precedent Images
LOCATION	Town Centre (Historic Focus)	<ul style="list-style-type: none"> Linear urban centre (Fairtrade Town / Mirfield Show; Mirfield food & craft Fayre); Valley bottom/ riverside; Origins: - River / canal (boatyards) industry (wool textiles & malting); 	<ul style="list-style-type: none"> High Street, Community uses, public realm areas; Frontage; Car parking; Townscape; shopfronts Public realm links; Slowing traffic and supporting strategic pedestrian & cycling; 	<ul style="list-style-type: none"> ✓ - Mixed-use frontage; Brownfield Land; Surface parking; Conversion of historic asset; Heritage led / Canalside sites Affordable housing; ✗ - Box retail; low density development 	
	Urban Neighbourhood (Riverside)	Former industrial urban housing and community focussed along Calder river by Ledgard Mill	Riverside frontage & connectivity; Civilised streets - parking and landscaping	<ul style="list-style-type: none"> Riverside frontage (mixed-use); Urban infill (terrace); Conversion (works); 	
	Suburban Neighbourhood (& local centres)	South-facing hillside communities; suburban streets, historic villages; local centres; schools, open spaces & insitutions	<ul style="list-style-type: none"> Supporting historic focus areas and maintaining green corridors / views; Connected walls & consistent boundaries 	<ul style="list-style-type: none"> Infill sites; Backlands development; Urban extensions; 	
	Industrial (valley bottom)	Vallery bottom; flood plain; gateways; green infrastructure and productive land	Safe streets and spaces connecting the station; green infrastructure connectivity	<ul style="list-style-type: none"> Regeneration of brownfield land; Flood plain sites Converted industrial; Transport orientated development; Employment uses; green infrastructure 	
	Upper Hopton (Conservation Area)	<ul style="list-style-type: none"> Semi-rural hillside village; Origins: - farming & industry (wool textiles, agriculture & malting); Church & community centres 	<ul style="list-style-type: none"> Lanes; Walls, boundaries, mature trees / hedgerows and footways; Increased space/ support for walking & cycling; 	<ul style="list-style-type: none"> Innovative isolated plot; Traditional infill plot; Conversion or extension of agricultural building; Affordable housing; 	
	Semi-rural (hamlet/ farmsted)	Open and wooded hillsides; isolated farmsetds and hamlets; countryside access & unspoilt views	Protecting green belt and small scale sustainable & affordable growth	<ul style="list-style-type: none"> Green-belt / green field; Innovative isolated plot; Conversion or extension of agricultural building; Affordable housing; 	

4.8 Form, scale & massing













Scale, mass and form are generally interlinked. The scale of a building refers to its height, length and width. The mass is its overall shape and size, considering the building scale and volume. The form is more than the 3D envelope of the building (describing the general parameters but not the detailed form) and includes the floor plan and extrusions such as porches or roof form.

Developing larger buildings will be most appropriate in areas where such buildings are characteristic, for example in the former industrial lower valley bottom where mills, workhouses and workshops were common.

Where larger scale buildings do exist, new development should be informed by an understanding of their historic form. This can range from symmetrical and formal to informal compositions with varied projections and rooflines.

New development should respect the scale and simplicity of smaller scale buildings where these are common, avoiding elements such as projections and bay windows which may diverge from this local character. All major design elements should be considered together (porches, entries, eaves, facades and chimneys).



FORMS AND FEATURES				
	Characteristic features	Positive / Negative features	Traditional examples	Modern Examples
APARTMENT	<ul style="list-style-type: none"> Dual aspect apartments; Part of perimeter block; Set close to back of pavement Mixed-use ground-floor; Fronting street/open space; 	<p>✓ - Well proportioned openings (giving daylight to floorplans); Balconies break up big facades</p> <p>✗ - Monotonous facades; mean openings; not giving street enclosure</p>		
TERRACE	<ul style="list-style-type: none"> Part of perimeter block; Set close to back of pavement; Fronting street/open space; Balconies break up big facades; On street parking (or small walled courts/lanes to the side) 	<p>✓ - Well proportioned openings; Ginnel access; rear access paths for bins</p> <p>✗ - Long rows; monotonous facades; mean openings; parking to front of plot</p>		
TOWNHOUSE	<ul style="list-style-type: none"> Flexible ground floor use; Rear parking access/lane; Attached form; Paired; Larger individual 	<p>✓ - Increased height proportionate to set back on plot (for enclosure); bay windows; recessed porches</p> <p>✗ - Integral garages (not on Main Streets); garage dominated frontage;</p>		
VILLA / BUNGALOW	<ul style="list-style-type: none"> Part of perimeter block; Set-back on plot; larger plot sizes proportionate to size of dwelling; 	<p>✓ - Porches; symmetry of façade; bay windows; strong form (sufficient width) / overhanging eaves; dormers.</p> <p>✗ - Integral garages on Main Streets; garage dominated frontage; (roof pitch min. 35' to for accommodation in roof space if 1 st.)</p>		
MEWS-HOUSE	<ul style="list-style-type: none"> Part of perimeter block; (facing Minor Streets), Interior courtyards & back lanes; Commonly attached or individual; 	<p>✓ - Integral garage; accommodation above in 1st floor and roofspace</p> <p>✗ - Carriage arches to rear court; narrow street space; over-basic design;</p>		
CARRIAGEHOUSE / BARN	<ul style="list-style-type: none"> A: Carriagehouse: - detached or part of courtyard or linear block B: Barn: - Part of complex; courtyard or detached 	<p>A Carriagehouse</p> <p>✓ -</p> <p>✗ - lack of plot definition / enclosure</p> <p>B: Barn</p> <p>✓ - velux windows; low-level / overhanging eaves; connected walls;</p> <p>✗ - limited/narrow openings; carriage-arches to rear parking;</p>		

4.9 Building heights and roofscape

The height of a building and its overall mass important factors in determining the impact a building will have on its surroundings. This includes; perception of the the street scene; by neighbours, and how it fits into the character of the neighbourhood.

The building should allow sunshine to private outdoor spaces and minimise overshadowing and overlooking of adjacent properties. This is a particular issue for developments on back land.

- Consider two storey developments on regular plots to create more usable outdoor space.
- Consider single storey developments on rear sites to minimise overlooking.







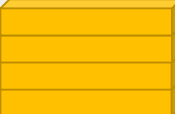







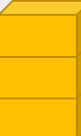





- On sloping sites, consider stepping the sections of the house up or down the slope to minimise earthworks and reinforce the natural topography.

Where it is necessary for a new building to be taller than the surrounding buildings, the building should be designed sensitively to minimise its impact. This might be through the use of set back and projecting sections where appropriately characteristic, or by taking advantage of height changes in topography.

Mirfield rises up the valley on both sides and roofs are often highly visible. The roof design is important for integrating the buildings into the neighbourhood or landscape. Think carefully about how to achieve this and create a roof design that is sympathetic to the context, street and neighbourhood.



Figure 73: Table of building heights by character area

		Building heights	Max. difference to neighbour (storey)	Roofscape features (e.g. chimney/spire)	Illustrations
LOCATION & BUILDING HEIGHTS	Town Centre (Historic Core)	2 - 3.5 st 	plus 1 storey	Plus 1 storey elements: Spires; towers; dormer turrets; projections & chimneys *May break roofline for special buildings	
	Urban (Riverside)	2 - 3 storeys 	up to plus 2 storey	Plus 0.5 storey elements:- mansard roofs; towers, dormer turrets, projections & chimneys *May break roofline for special buildings	
	Suburban (Hillside)	1.5 - 2.5 storeys 	up to plus 0.5 storey *up to plus 1 storey around local centres	✓ - Chimneys; dormer windows; solar panels *Additional features allowed at local centres, (e.g. spires; towers; dormer turrets; projections)	
	Post-Industrial (Valley bottom)	2 - 4 storeys 	up to plus 1 storey	Plus 1 storey elements: Spires; towers; dormer turrets; projections & tall chimneys & mansard roofs *May break roofline for special buildings	
	Conservation Area (Hillside)	1.5 - 2.5 storeys 	up to plus 0.5 storey	✓ - Chimneys, eyebrow dormers ✗ - No boxed dormers facing the street	
	Semi-rural (Greenbelt)	1.5 - 2 storeys 	up to plus 0.5 storey	✓ - Chimneys, eyebrow dormers	
STREET & EDGES	Prominent sites or locations (e.g. end of street vista, corner site of 2 main streets)	✗ - 1 storey 	up to plus (or minus) 1 storey	Plus 1 storey elements eg. Spires, Towers, Chimneys *May break roofline for special buildings	
	Main streets (axial)	2 - 3 storeys 	up to plus 1 storey (dormers allowed)	Plus 1 st. elements: - Spires, towers, dormer turrets, projections & chimneys	
	Minor Streets	1.5 - 3 storeys 	up to plus 0.5 storey (within roof-space, dormers allowed)	Plus 0.5 st. elements: - Spires, towers, dormer turrets, projections & chimneys	
	Edge of Settlement	1.5 - 2 storeys 	up to plus 0.5 storey (within roof-space, non-box dormers allowed)	Plus 0.5 st. ✓ - Chimneys; eye-brow dormers	

4.10 Conversions and alterations

A good conversion should result in the minimum alteration to the external appearance of the building. Note issues of maintaining boundary walls, impact of parking, access requirements (new doors can unbalance the design of a facade and provision of outdoor space for homes).

In areas where decorative details are common, when these are to be replaced or where new openings are added, the decorative details should copy those on the original building or reflect them in some way. This will ensure continuity between openings and maintain the unity of the building. Avoid creating new detailing or decoration on simple buildings where it would traditionally not have existed.



Conversions in conservation areas should be of a standard and type of design compatible with the character of the area. Conversion may require extension but these should not overwhelm the original building.



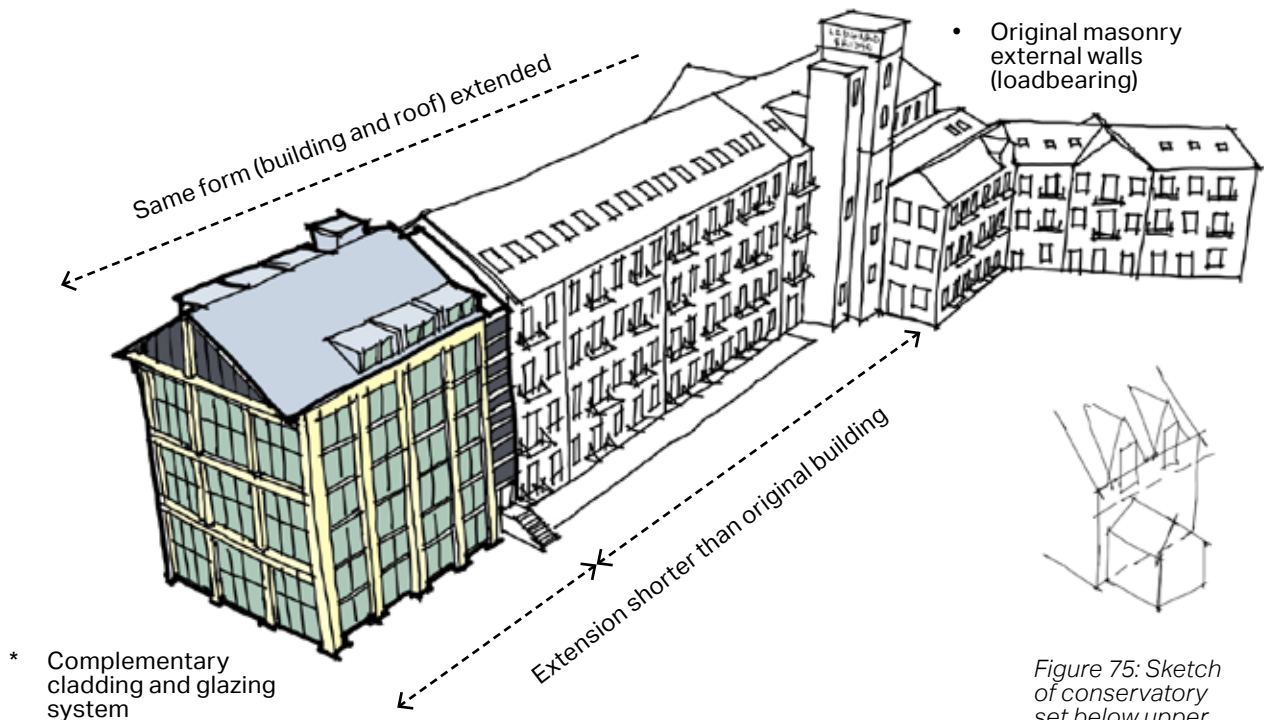


Figure 74: Ledguard Bridge Mill, Ledguard Wharf

Figure 75: Sketch of conservatory set below upper story window sills

4.11 Extensions and conservatories

Well-designed extensions can revitalise older buildings and contribute positively to local character. If done badly, extensions detract from the original building and can impact on the wider local landscape. It is essential to ensure that an extension has strong unity and a relationship to the original building, strengthening character rather than weakening it. This does not mean that extensions should copy existing development: sensitive and good modern design can complement the original building and respect local character.

The general size, height and width of the extension should normally be less than the original building, ensuring that it remains similar or subordinate to the original building in terms of scale and form.

Consider the appropriate building methods, colours and architectural styles for the extension. These can be traditional or contemporary as long as they complement the original building and local character. It may be most appropriate for extensions on significant or notable buildings to be clearly different from the original building. This can allow the merits of the original building to stand out. However such a decision should always be based on an understanding of the building's character.

On a two storey building the conservatory should normally be no higher than the underside of first floor sills of the original building. It is usually preferable to locate conservatories on the rear of the house.

4.12 Materials & details

Natural building materials that are found locally and are not man-made should be used wherever possible on buildings. This is especially the case if the building is listed or in a Conservation Area. Mirfield has a strong core palette and this should be considered as a baseline to work with.

New building materials are not discounted but must complement the local vernacular. Innovation with purpose and sustainably sourced materials are good practice and will help to minimise resource usage and keep supply chains local, saving energy.

High-quality materials are important for all buildings and must be of a standard to last as buildings can endure for a long time. They must be maintainable and also delightful in order to encourage people to retain and repurpose the built

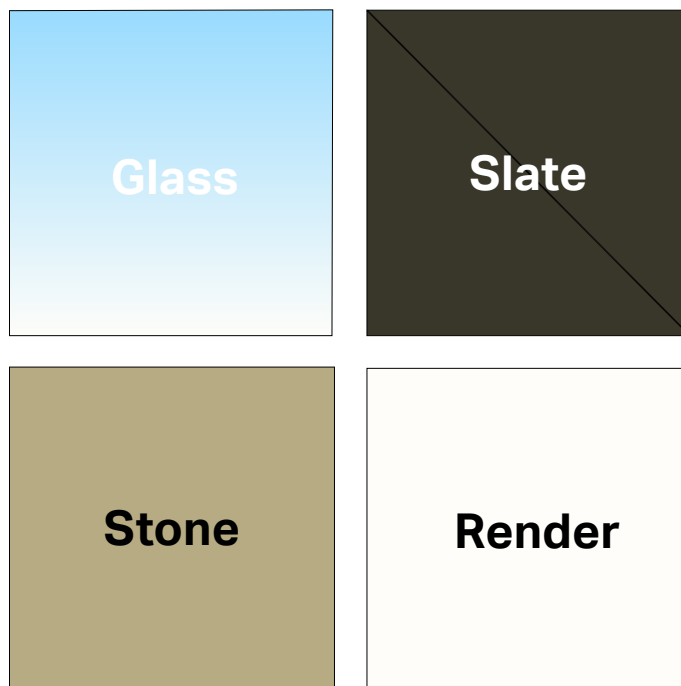


Figure 76: Typical material palette in Mirfield

environment over time, as we have learnt to do with our historic buildings. A good building will have an impact on everyone who passes it.

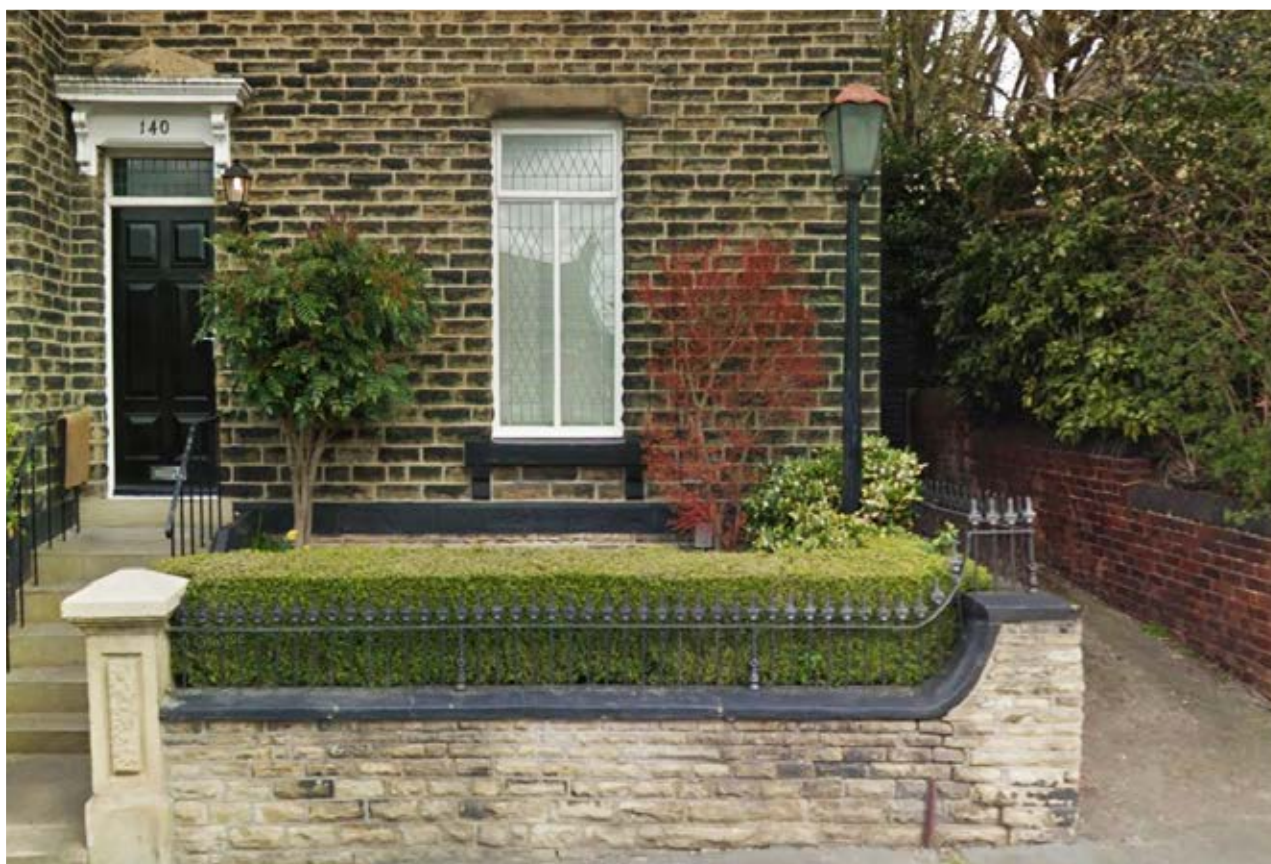






Figure 77: Table of materials and details in the Town Centre Area

		Figure 77: Table of materials and details in the Town Centre Area			
		HISTORIC CORE (inc. Town Centre)			
		Common	Uncommon	Negative	
LOCATION	Materials	Roofscape	<ul style="list-style-type: none">Stone SlateBrick chimneyStone decoration	n/a	<ul style="list-style-type: none">Gaudy / oversized shop signsStreet clutter, signage and guard rails;Modern, generic shopfronts.
		Elevation	<ul style="list-style-type: none">Sandy brick / stoneWooden framesGlazing	<ul style="list-style-type: none">Ashlar stoneRender	<ul style="list-style-type: none">UPVC window framesRed brick facing streetPainted stonework
		Boundaries & Garages	<ul style="list-style-type: none">Sandy Brick / stoneDry stone / rusticationIron railings atop walls	n/a	<ul style="list-style-type: none">Not matching neighbouring or primary building;Red brick / painted
			Positive		Negative
			Common	Less common	Discouraged
	Details & Features	Roofscape	<ul style="list-style-type: none">Gable, Hipped;Half-gable end;Chimneys	<ul style="list-style-type: none">Mansard,PedimentsFinnialsGabled dormer windows	<ul style="list-style-type: none">Box dormers
					
		Façade	<ul style="list-style-type: none">Bay / sash windows; decorative surrounds;Stone mullions; Quoins;Porches / simple canopies;	<ul style="list-style-type: none">DentilsPorticosTraditional shopfrontsBalconies	<ul style="list-style-type: none">Non-traditional shop-frontsOversized / lit signageReplacement windows
					
		Garages	<ul style="list-style-type: none">Not in Town Centre	<ul style="list-style-type: none">Found with some residential properties	<ul style="list-style-type: none">Behind main building;Gable roof runs counter to main building
					
	Boundaries	<ul style="list-style-type: none">Regular brick / stone connecting buildings, defining public realm;Drystone, piers to park	<ul style="list-style-type: none">Decorative ironwork, gates and railings atop walls	<ul style="list-style-type: none">Missing railings or not matching	
					

4.13 Layout & Topography

This aspect is key to Mirfield's sense of place. Maintain the connection to the landscape and sense of the valley in this settlement by relating buildings to the sloping topography. Relate built form to the sloping landform and topography by stepping or extending storeys down or up slope. Consider how the orientation of streets, blocks, terraces, buildings facades and roofscapes help to read or reinforce the sense of topography and respond to traditional building patterns that align to, or run counter to the valley's slopes and folds.



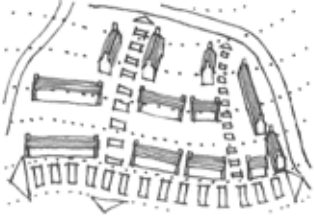


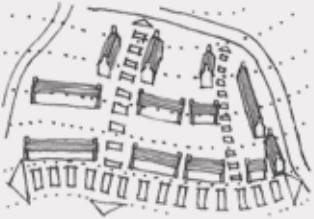

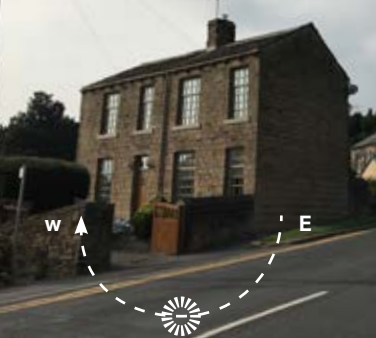




	Considerations	Street / Block	Main façade (main entrance and openings)	Roof alignment (if independent)
LAYOUT & TOPOGRAPHY	Angle to the street:	n/a	<ul style="list-style-type: none"> ✓ - Parallel or 90° with lane/path access (typical of area) ✗ - Avoid a 'sawtooth' alignment unless responding to site levels or for solar orientation 	<ul style="list-style-type: none"> ✓ - Parallel or 90° to match or oppose façade/street; ⦿ - Custom angles may create a less harmonious roofscape;
				
	Angle for solar orientation:	<ul style="list-style-type: none"> ✓ - Longest streets to run within 0-30° of east-west, subject to levels to optimise building orientation for daylighting & solar gain 	<ul style="list-style-type: none"> ✓ - 0-30° of east west (optimises daylighting) 	<ul style="list-style-type: none"> ✓ - 0-30° of east-west (optimises solar panel production potential) ✗ - North-facing mono-pitches (low capacity); ⦿ - North facing rooflights provide even daylighting
		 		
	Alignment to / topography (east-west valley):	<ul style="list-style-type: none"> ✓ - Retain minimum cross-fall on streets for drainage ✗ - Do not exceed maximum gradient on streets (1-12) for pedestrian comfort 	<ul style="list-style-type: none"> ✓ - Longest side to run within 0-30° of east-west ✗ - Longest side to run counter to topography (unless stepped) 	<ul style="list-style-type: none"> ✓ - Longest side to run within 0-30° of valley topography (efficiencies / responding to reinforcing the urban-landscape character)
				

Figure 78: Table of layout and topography responses



5. CHARACTER AREA DESIGN CODES

5.1 Character Areas

Having analysed the spatial form of the settlement in part 2: Parish Assessment and the Historic focus areas in part 3: Local Distinctiveness the following character areas are defined for purposes of applying the guidance and codes:

- Riverside urban (Lower Hopton);
- Historic Core: Mixed-use town centre;
- Calder Valley Former Industrial;
- Northern Suburbs and local centres;
- Rural villages: E.g. Upper Hopton; and
- Rural hinterlands: Farmsteads & hamlets.

These are based on assessment of both urban and landscape character attributes.

5.2 The Design Codes

The following codes are proposed for the character areas and take on board essential basics and timeless principles of urban design.

The aim is to retain, improve or recover good character wherever possible across Mirfield. Local distinctiveness can be seen in local character areas aswell as the whole parish.

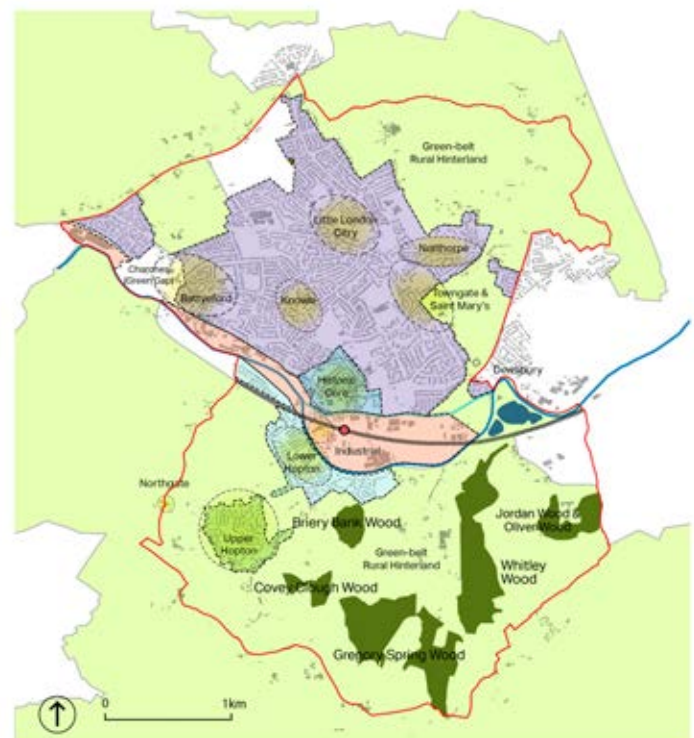


Figure 79: Character Areas and Historic Focus Areas map



Figure Ground Sample

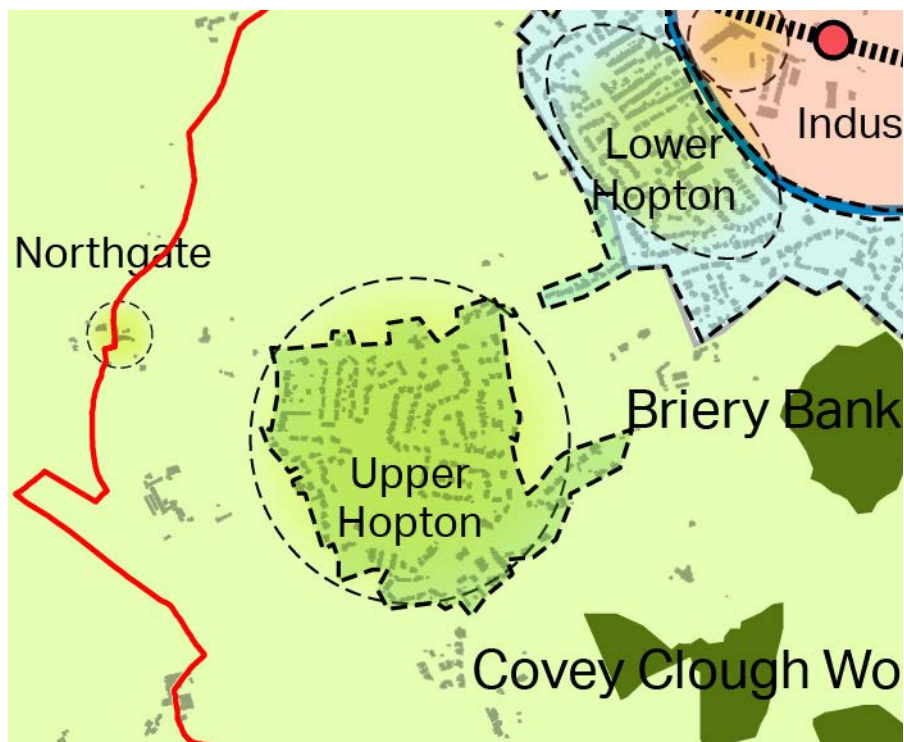


Figure 80: Character Area Code Locator

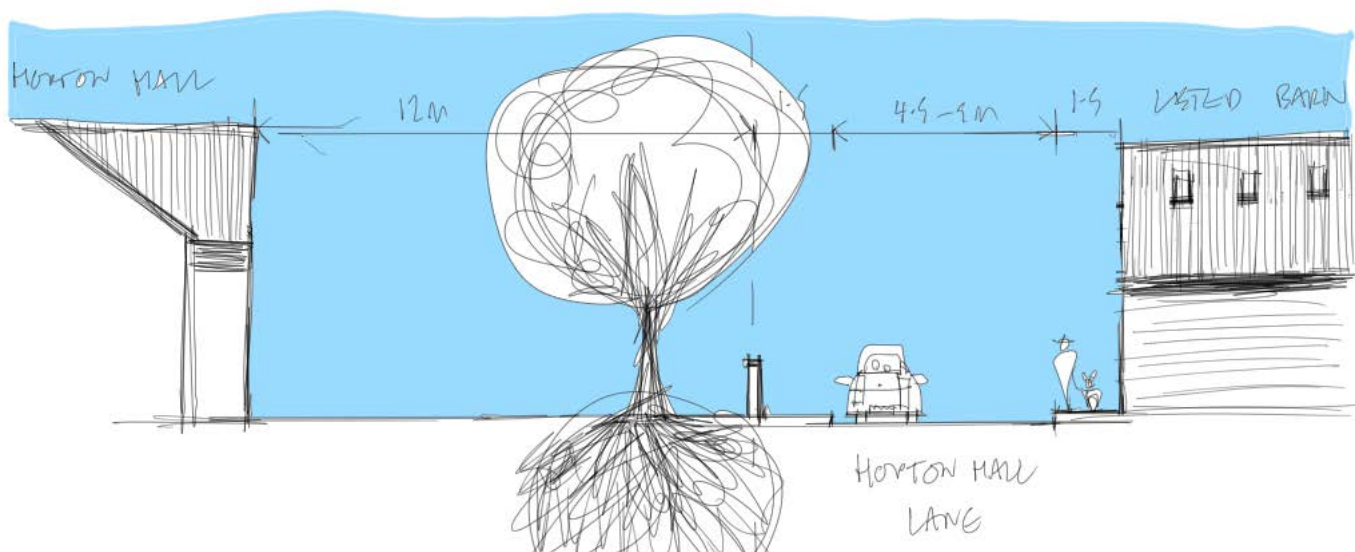


Figure 81: Hopton Hall Lane Street Section

CA1 - Upper Hopton (inc. Conservation Area) Design Codes

The purpose of the Conservation Area designation is to preserve the Character, Appearance and Special Interest of the Conservation Area and as such that is the purpose of this Design code, whilst also encouraging high-quality design. The design codes are closely related to the Conservation Area Appraisal and Management plan and should be read in conjunction with this document and local planning policy.

The historic character, appearance and special interest of the Upper Hopton conservation area has been assessed in Section 2 (2.8) and aims to reinforce the conclusions and suggestions of the Management plan by suggesting methods and codes to; preserve the character, appearance and what is valuable in the conservation area; assist in managing change and allow the sympathetic evolution and enhancement of the conservation area.

Retain and reinstate character / architectural features:

Reinstatement of features is very important for the settlement. Some buildings within the Conservation Area have lost their original doors, windows, boundary walls, roof materials and other features. As a result, their character is being eroded, therefore when proposals for alterations and extensions are submitted there will be an opportunity to encourage the reinstatement of lost features to reflect their original pattern, style and material.

Materials and details:

It is generally preferable to use locally characteristic materials as they provide a link with the local landscape. The use of traditional materials, replacement of original styles of windows and doors are encouraged in submissions for planning permission. New development must use the predominant traditional materials as a starting point although complimentary materials like timber, glass, steel and aluminium may be explored as befits the evolution of the area towards ensuring a highly sustainable place is achieved.

Replacing timber windows and doors with basic plastic alternatives will impact on the conservation area and is advised against. It is recognised that it can be costly to use high quality material replacements but this is preferred and helps preserve character.

Protect open spaces:

The open space in the settlement area consists of private gardens, the recreation ground and the Sutcliffe Memorial Ground (cricket ground), and a number of fields which surround the village. Too much infill development and extensive housing developments in Upper Hopton will have a serious effect on the character of the conservation area due to the elevated location of the village. Urban sprawl, as has happened to the north of the Parish would result in further coalescence of settlements which would harm Upper Hopton's sense of rurality.

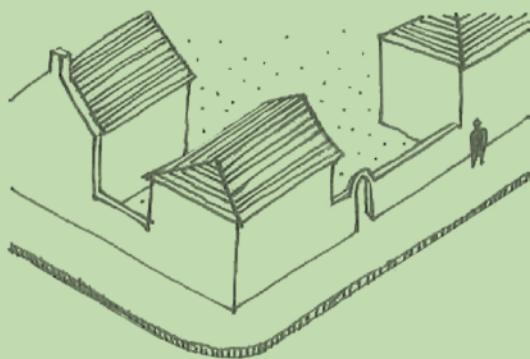
See also figure 28 in section 2.8.

Trees and TPOs:

Section 211 of the Town and Country Planning Act 1990, requires that anyone proposing to cut down, lop, or top a tree in a conservation area must give six weeks notice to the local planning authority. The purpose of this requirement is to give the authority an opportunity to make a tree preservation order. Trees make an important contribution to the character and appearance of the conservation area and its setting. There are already some tree preservation orders on individual trees and groups of trees in the Upper Hopton Conservation Area.

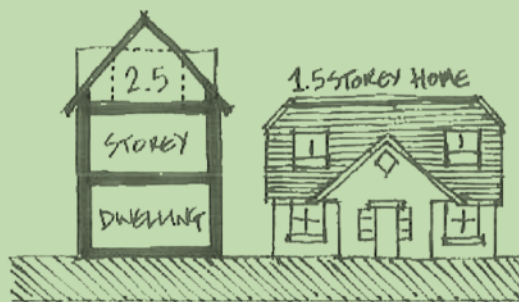
Defining the public realm and private plots with boundaries:

Loss of boundary walls and/ or planting may be harmful to the character and be detrimental to the conservation area. For example, knocking down walls to form parking areas will have a clear detrimental effect on the character and appearance of the street-scene and conservation area. Boundaries to public and private space should be clearly defined by one of the following features; hedgerows, walls or estate railings/ fencing facing towards rural edges. Knocking down walls to form parking areas has a clear detrimental effect on the character and appearance of the area and must not be allowed.



Building height and scale:

2 storey dwellings are common, with some single storey extensions or out buildings. Occasional 1.5 and 2.5 storey ones should be allowed in key locations to hold views and allow a varied roofscape. However, consideration of actual storey and building heights must be considered as some older, rural buildings will have lower floor to ceiling heights so should not be dwarfed by new additions. Likewise where greater floor to ceiling heights are present in existing buildings, squat new development would not relate well or enhance present proportions.



Minor alterations:

In the conservation area some developments (which are usually “permitted development” outside) are not always classified as “permitted development” within. This includes various types of cladding, the insertion of dormer windows, raising the ridge or expanding the size of a roof and the erection of satellite dishes on the walls, roofs and chimneys which front a highway.

Also, within a conservation area the size of extensions that may be erected without specific planning permission is more restricted. If the extension and any previous extension exceeds in total 50 cubic metres or 10% of the house volume it will require planning permission.



Building and protecting views:

The relationship of new building and the landscape must be considered at various scales to understand how the building will affect the landscape and views from nearby and far away. Carefully consider the position of new development in relationship to key views into and out of the village, along lanes and to key buildings. Take steps to ensure that a building ties into the local landscape and does not detract from key views.

Public realm & hard landscape:

A vast amount of the historic floorscape has been lost over the years. It is important that when any road works occur that stone flags, setts or kerbs are retained and replaced instead of replacing with modern materials such as tarmac, concrete flags and kerbs. Speed control measures should also respect the historic character of the conservation area and attempt to blend in with the materials used in the Upper Hopton public realm. The the mini-roundabout junction of Jackroyd Lane and Hopton Hall Lane, and adjacent tarmac public space could be treated holistically, resurfaced and beautified to match the quality of the planting schemes.

Signage and road markings:

Street signage should be minimal so that it does not detract from the character of the area. Street furniture ranging from street-lamps to bollards, bins, and benches should be carefully placed so that they do not clutter the streets and harm the appearance of the Upper Hopton Conservation Area. A standard selection of furniture, possibly from a general palette for wider application and a special palette for key locations will help to generate and tie in public realm.



Opportunities to enhance:

Any further street improvement schemes in Upper Hopton should respect the historic nature of the village and should enhance the conservation area visually as well as functionally.

Historic Building Grants may be available towards the repair or restoration of historic buildings within Conservation Areas or Listed Buildings. These grants are means tested and are for structural works. The owner must also have been in residence for over two years.



Figure 86: Character Area Code Locator

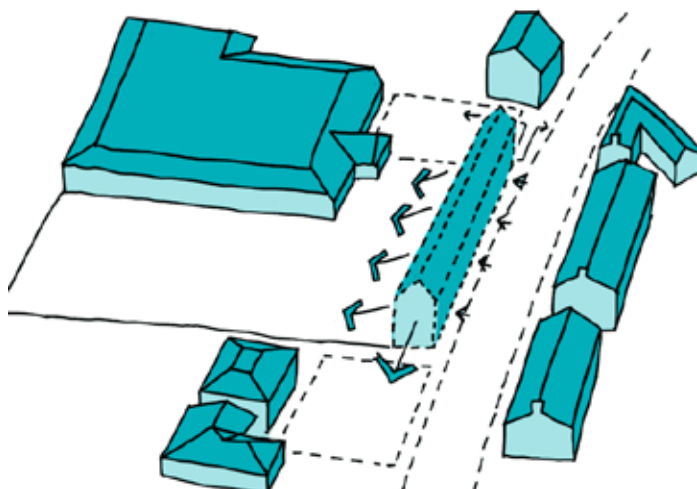


Figure 84: Indicative sketch of how a small-scale development opportunity can create frontages to key streets, small retail units for local businesses and improve overall attractiveness of the town centre by shielding parking, associated with 'big box' retail.

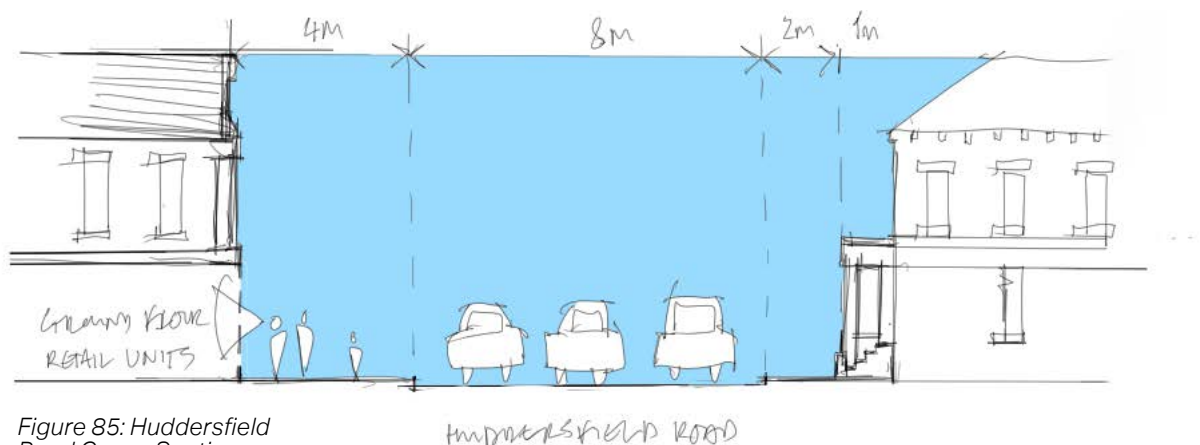


Figure 85: Huddersfield Road Cross-Section



Figure 82: Figure Ground sample
Figure 83: Birds-eye aerial view

HC1 - Historic Core (inc. Town Centre) Design Codes

The historic character, appearance and potential special interest of the Historic Core has been described and assessed in Section 3.2 of the report. This design code will also suggest methods and codes to preserve the character, appearance (and reinstate) what is valuable in the area (or has been lost), assist in managing change and the sympathetic evolution of the town centre (and potential future conservation area) as well as suggesting some proposals for its enhancement.

Reinstate character features and traditional shopfronts:

Reinstatement of features is very important for the town centre area. Some buildings within the area have lost their original doors, windows, boundary walls, roof materials and other features. As a result, their character is being eroded, therefore when proposals for alterations to shopfronts and frontages or residential applications are submitted there will be an opportunity to encourage the reinstate traditional features and shopfronts to reflect their original pattern, style and material.



Materials and details:

The use of traditional materials, replacement of original styles of windows, doors and shopfronts are encouraged in submissions for planning permission.

The oldest parts of Mirfield, pre 1850s, are characterised by the use of locally quarried stone and include the classic York Stone Flag roof (see also figure 79 Table of Materials).

Avoid use of UPVC shopfronts, window and door frames. This is a high embodied energy material that is not sustainably produced but the recycling of existing UPVC is important.

Minor Alterations:

Any work that materially affects the external appearance of a building requires planning permission, subject to "permitted development" rights. Under normal planning control certain works are classed as "permitted development" and do not require planning permission. This includes small alterations and extensions to dwelling houses, the erection of buildings, enclosures or pools required for a purpose incidental to the enjoyment of a dwelling house, the provision of hard surfaces, and the erection of and alterations to boundaries.

Minor alterations which do not require planning permission can still have an impact on the character of the area. Replacing timber windows and doors with the plastic alternative and knocking down walls to form parking areas have a clear detrimental effect on the character and appearance of the area.

Open spaces:

The principal open spaces on the periphery of the town centre area consists of the Mirfield Showground, Mirfield Memorial recreation ground and Ings Grove Park as well as private gardens. Too much infill development and extensive housing developments in will have a serious effect on the character of the town centre area due to the need for green infrastructure in the heart of the settlement that offsets the higher density of development that is occurring here. Further encroachment here will deprive Mirfield of it's 'green lungs' which help to offset the pollution of through traffic and soften views from up on the hill-sides.



Street Trees and Planting:

Trees make an important contribution to the character and appearance of the town centre and its setting. There are already some tree preservation orders on individual trees and groups of trees in the Town Centre vicinity. When considering development sites where trees are existing retention of large trees must be the first option. New development can incorporate old trees, giving instant maturity and added value that younger trees take many years to achieve. New street trees are vital for addressing future climate change.

Boundary definition:

Loss of boundary walls and/ or planting may be harmful to the character and be detrimental to the town centre area. For example, knocking down walls to form parking areas will have a clear detrimental effect on the character and appearance of the street-scene and sense of enclosure.

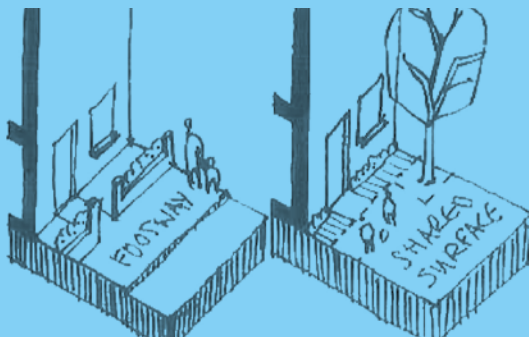
New development must consider walls to connect the built form and distinguish public from private space where this is not done by the building itself. Open forecourts with landscaping for public buildings, such as at the library, greatly complement this approach by recognising importance..

Building height and scale:

Typically 2.5 - 3 storey residential and 3-3.5 storey mixed-use buildings are best. Some four storey buildings may or elements may be appropriate in key locations to hold key views and allow a varied roofscape. However, consideration of actual storey and building heights must be considered as different periods of buildings will have varying floor to ceiling heights so new additions must be sympathetic to this subtlety. New development must relate well not just in terms of height but depth and width, particularly providing a regular rhythm of interesting facade detail at street level (e.g. adaptable as shopfronts) and complement proportions in existing character buildings.

Public Realm & character:

A vast amount of historic floorscape has been lost over the years in Mirfield town centre and around. It is important that when any road works occur that stone flags, setts or kerbs are retained and replaced instead of replacing with modern materials such as tarmac, concrete flags and kerbs. Speed control measures should also respect the historic character of the urban core and attempt to blend in with the materials used in the town centre and riverside public realm.



Streets and spaces:

Streets are for both a place and movement function. Mirfield High Street has over time squeezed space for pedestrians and cyclists whilst wider junction radii have imposed the dominance of through traffic. More balanced and civilised streets are required in these areas of business and identity. Lots of traffic passing through can be good for business but it must do so in a slow and orderly way that does not compromise pedestrian use and comfort. Crossing between shopfronts must not involve risking your life for traffic.

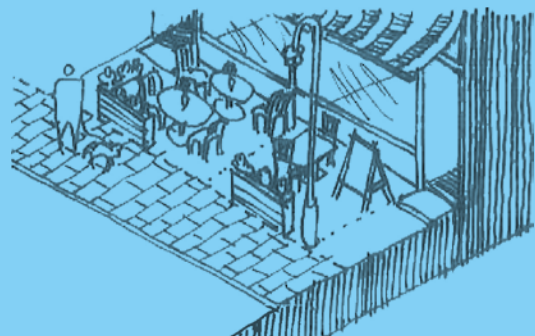
Any further street improvement schemes in the town centre / riverside should respect the historic nature of the centres and should enhance the conservation area both visually as well as functionally for all users putting pedestrians, wheelchairs and cyclists first.



Signage and street furniture:

The long term ambition should be that the character of Mirfield's streets and spaces does not require a host of signs, guardrails and physical controls for speed because drivers know they are entering a people first environment that they must pass through with an awareness of others.

Street signage should be minimal so that it does not detract from the character of the area. Street furniture ranging from street-lamps to bollards, bins, and benches should be carefully placed so that they do not clutter the streets and harm the appearance of the area. A standard selection of furniture, possibly from a general palette for wider application and a special palette for key locations will help to generate and tie in public realm. This must be integrated or replaced with landscaping that softens character and brings a sense of place.



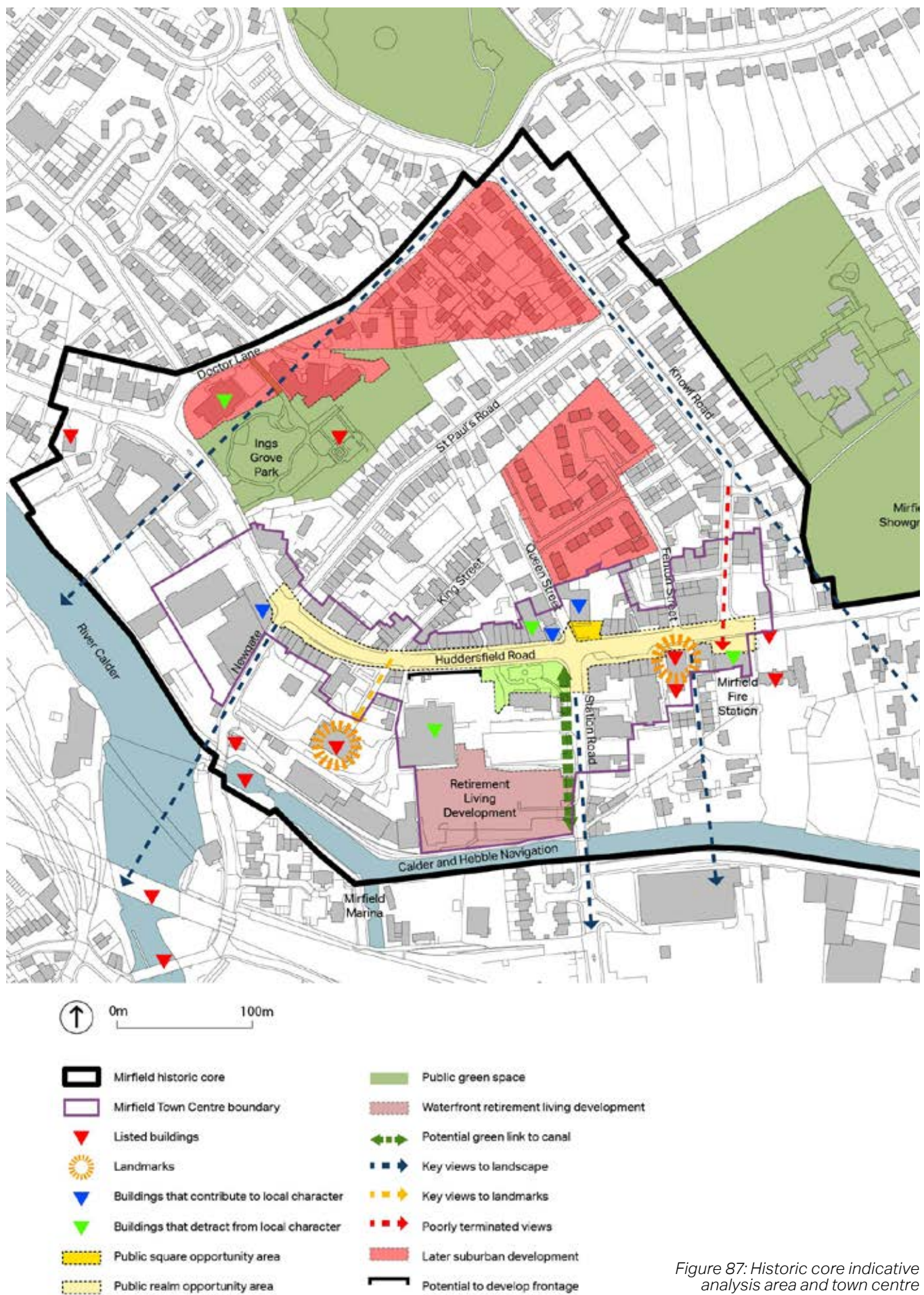
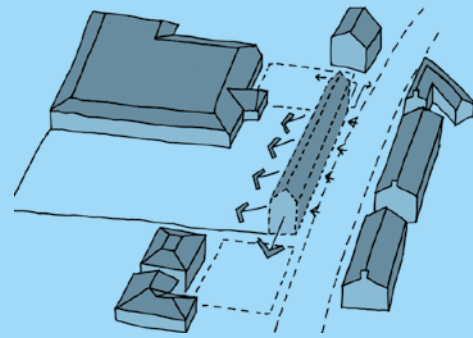
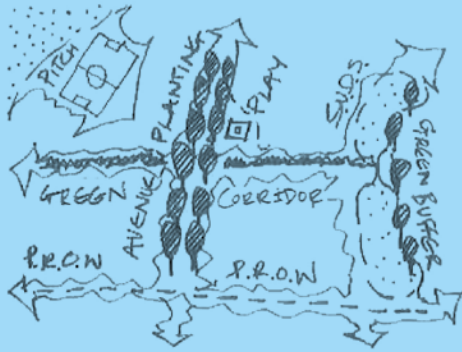


Figure 87: Historic core indicative analysis area and town centre



Key opportunities to consider (see plan on previous page):

- 1. Green link to Station** - There must be a clearer strategy to make a green and pleasant route to the station. New tree planting within this street corridor could create legibility and make a much more attractive route from the station to the town centre. The street is broad and so even without a clear approach to development frontage there is a landscaping opportunity to seize.
- 2. Public square opportunity area (Tesco)** – the setting to this important ‘header building’ (terminating the vista from Station Road) is eroded due to the piecemeal and irregular hard-landscape design, particularly clutter including signage; lighting bollards and bins which could be consolidated. The disabled parking bays could be reprovided discreetly on a higher quality hard-landscape surface (with flush block paving rather than tarmac and white lines which degrade the setting and highlight car dominance).
- 3. Co-op site edge Huddersfield Road**
The lack of building along this edge and views over an expansive car park lets down the quality of the townscape with some poor views over surface car parking and the single-storey Co-op building (despite the lovely valley backdrop). Glimpsed views to the valley and car park could be retained to maintain legibility but the overall frontage on this outlook would help to improve the high street if it could be redeveloped in a more considered fashion. The Co-op building is an example of the type of box-retail building surrounded by car-parking that should not be allowed in a largely traditional and human-scale town centre.
- 4. Public realm opportunity area (Huddersfield Road)** – there is potential to improve the hard landscape and introduce planting and traffic calming to reduce vehicle speeds and create a more comfortable pedestrian environment (and allow traffic to flow). E.g. consider wider pavements; planting between on street parking; higher quality finishes at key junctions to enable level-crossing for pedestrians and wheelchairs.



Figure 91: Character Area Code Locator

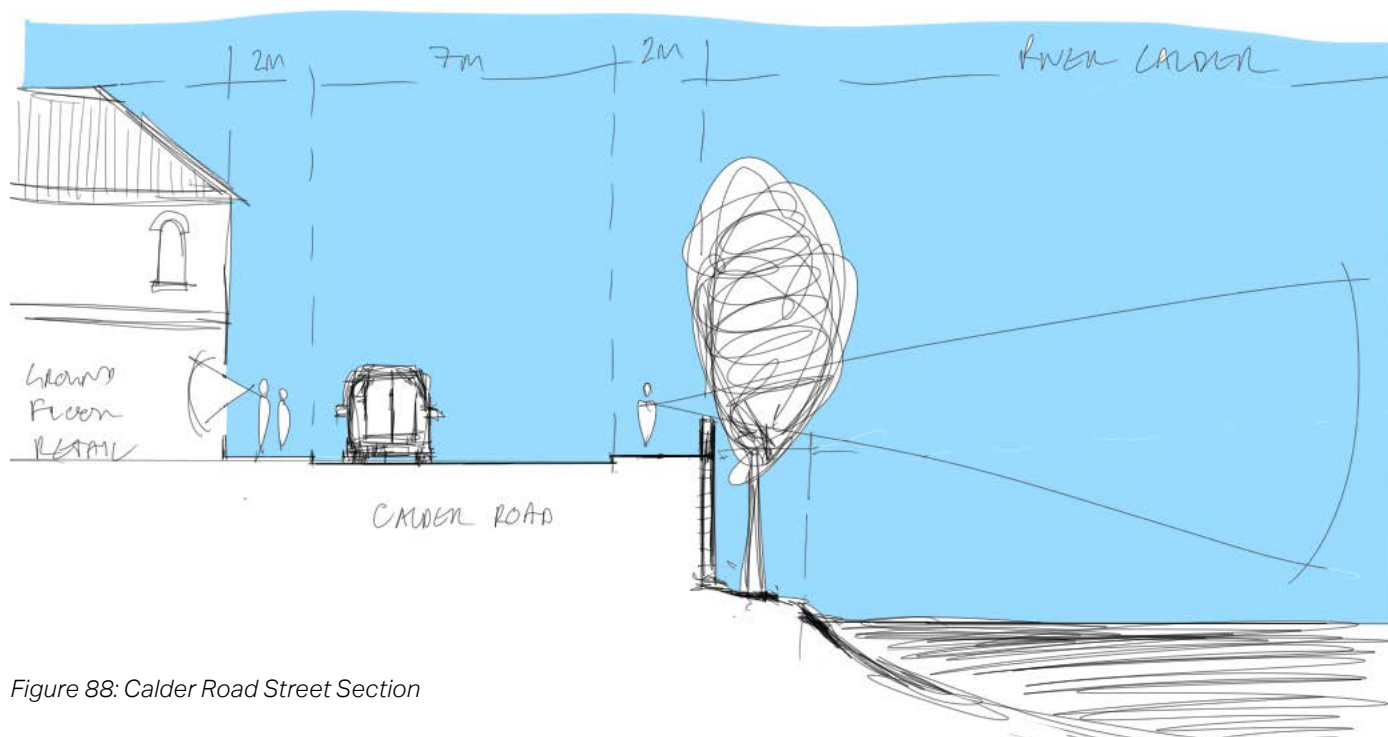


Figure 88: Calder Road Street Section



Figure 89: Figure Ground sample

Figure 90: Birds-eye aerial view

RU1 - Riverside 'Urban' Suburb (Lower Hopton)

A Victorian grid of streets and terraces (with some C21 suburban extensions) gives this riverside neighbourhood its distinctive character. There is a strong structure of urban-grid streets laid out perpendicular to the riverside frontage which is also the area's local centre and main thoroughfare (Calder Road), a key face of the neighbourhood (and town) which should be valued greatly for its outward looking terraces and shops.

Residential and Industrial Focus: This neighbourhood expanded significantly to provide housing for the valley bottom textile and other industries; the Victorian streets are aligned to the nearby Mill at Ledgard Bridge. There is a strong character to respond to;

Edges condition: The boundary with the River Calder is particularly important with the street/ footpath running alongside enabling views and landscape character to permeate the neighbourhood giving a very pleasant outlook. Development must address this;

Mixed-use flexibility: Developments should prove flexible and adaptable like the existing terraces facing the River Calder have - their ground floor allows for shops and cafes which should be supported by an improved public realm and space for pavement culture;

Building heights/ storeys: Varies in scale from two storey terraces, to 3 storey mixed-use townhouses on Calder Road. Bungalows (or which there are some already) should not be placed facing this main street as they are unsupporting of this important character frontage and streets hierarchy;

Development opportunities: Residential infill and redevelopment of some terraced streets have been poorly realised with squat semi-detached homes that do not match the scale of the Victorian terraces. These undermine the character on key corners such as Johnson Street/ Calder Street;

Plot boundaries: Development may be set close to back of pavement with small privacy strips/planting or low masonry wall with or without hedgerows/railings; Boundaries visible from the street must be masonry (not wooden board fencing);

Built-form: Mostly attached terraces on main streets with semi-detached and detached suburban homes on modern infill estates; boundary walls connect buildings/plots on main/minor streets;

Gardens and plot sizes: It varies with building type and size but generally small gardens or yards for terraces, some with back alleys to give access for bikes/bins;

Development pattern: Due to the sloping topography streets are set perpendicular to the slope with terraces stepping up to give an interesting roofline;

Street hierarchy: A legible hierarchy of well-enclosed grid streets aligned back to the river would benefit from more green space for street tree planting;

Building line formality: This area has a strong building line formality which creates a good degree of street scale;

Building materials: A mix of rusticated brick and stone (sometimes blackened by smoke), slate, and occasional original wooden frame fenestration/ shopfronts.



Figure 92: Character Area Code Locator



Figure 93: Figure Ground sample

◁ Figure 94: Birds-eye aerial view



NS1 - NORTHERN NEIGHBOURHOODS

Patches of historic town building and significant modern suburban infill gives the northern neighbourhoods their distinctive characters. However there is still structure with an overarching suburban settlement pattern including; centres (small mixed-use clusters), main streets; minor streets; infill developments (on former estates) and neighbourhood infrastructure (schools and open spaces).

Historic Focus Areas: once many small settlements, neighbourhoods such as Battyeford is now incorporated within the Township of Mirfield. Like Upper Hopton, the area maintains it's own identity. The character that this area is distinctive and residents are keen to maintain it as such.

Edges conditions: Sites along the edges closely relate to rural Green Belt character also and must repond to Code R1 also;

Mixed-character: Future developments should be for the next century but should blend with what has gone before. Housing developers should look to the wealth of historical architecture in Mirfield first but innovation - with purpose - is welcome;

Bungalow estates: The trend for single storey buildings has left a lasting imprint on Mirfield's hillsides and whilst these have proved popular these types are unsuitable for main streets and centres next to Historic Focus areas, or alongside traditional forms.

Development Pressures: Residential infill and development of protected open spaces; edge of town sites in adjoining green belt (see Figure 1 also);

Plot boundaries: Rear boundaries should be masonry; Front boundaries should be low masonry wall with piers; and/or railings and hedgerows;

Built-form: Varies from attached terraces on main streets to semi-detached and detached suburban homes on modern estates; boundary walls connect up buildings on main streets / in centres;

Building heights/ storeys: Varies in scale from two storey terraces, semi-detached and detached suburban homes to 2.5 storey mixed-use townhouses in local centres;

Gardens and plot sizes: Varying with building type and size but generally larger detached houses should be set on a larger/wider plot; whereas terraces must have deeper gardens to compensate;

Development pattern: Due to the hillside topography an irregular grid criss-crosses according to desire lines across the area between historic settlements;

Street hierarchy: A legible hierarchy of characterful streets of different scales connecting directly to local centres and community infrastructure need to be maintained and replanted with trees;

Building line formality: This area has potential to have a variety of character with formal streets close to the station, town centre or Lower Hopton and more informal areas feathering out eastward;

Building materials: A mix of stone, slate, brick as well as more innovative use of metal and glass should be explored in harmony with the landscape character - potential for innovation, within context.



Figure 96: Rural development aerial photograph sample



Figure 95: Character Area Code Locator



R1 - RURAL & SEMI-RURAL EDGES DESIGN CODES

It is usually a given that within and in close proximity to the Greenbelt, the local planning authority will expect the quality of built form to be of high quality. Residential development would be of low-density; buildings would be sensitively designed, likely using traditional building techniques and materials reflecting the local vernacular and key characteristics. It is also assumed that there would be a strong, locally-appropriate and effective landscape framework, with siting, access, layout, scale, design and engineering work being landscape-led. This will help to ensure that the development achieves a good fit in the landscape.

Typical building types: Rural vernacular farm houses and cottages, period or manor houses; agricultural buildings and barns;

Building setback: Varies greatly between building set back on plot with screening or boundary walls and close set to back of lanes (possibly oriented at 90' or with some defensible space to the front;

Plot boundaries: Rear boundaries stock fence or drystone adjacent to lanes; Front: Drystone or low masonry wall with piers;

Built-form: Varies from attached rural cottages to lone farmhouses, walled courtyard or simple form additions to buildings (e.g. L-plan or extensions at 90');

Building heights/ storeys: 2 - 2.5 storeys typically with some single storey dwellings or extensions; storey heights vary by period;

Degree of enclosure: Isolated buildings with boundary walls (and planting) help to define focal points within the open agriocultural landscape;

Gardens and plot sizes: Varied plot sizes; Gardens often walled or fenced;

Development pattern: Linear lanes and farm courtyard/ agricultural complex arrangements and forms means no regular 'urban blocks';

Street alignment: Historic lanes aligned to local or historic features and topography. Lanes and rural roads with deflections based on former and existing field patterns that help slow driver speed;

Building line formality: Irregular / informal, some buildings oriented at 90' to the street. Stepping in and out but united by boundary treatments;

Corner buildings/ details: Small windows / simple working / decorative elements on secondary façades;

Street scale and character: Narrow and intimate scale streets and spaces are common; Single-sided or no footpaths on rural roads; and drystone walls help to maintain enclosure and boundaries which prevent livestock access.

Public realm: Informal lanes with widening and narrowing points , important to keep historic lanes unsurfaced for character purposes;

Building materials: Stone, slate, render; some variety in use of metal, wood and glass but mostly materials harmony in with the local landscape;

Typical details and features: Simple vernacular detailing in stone with stone sills and lintels and well-scaled porches.

See also: Adapting Traditional Farm Buildings Best Practice Guidelines for Adaptive Reuse (Historic England, 2017)

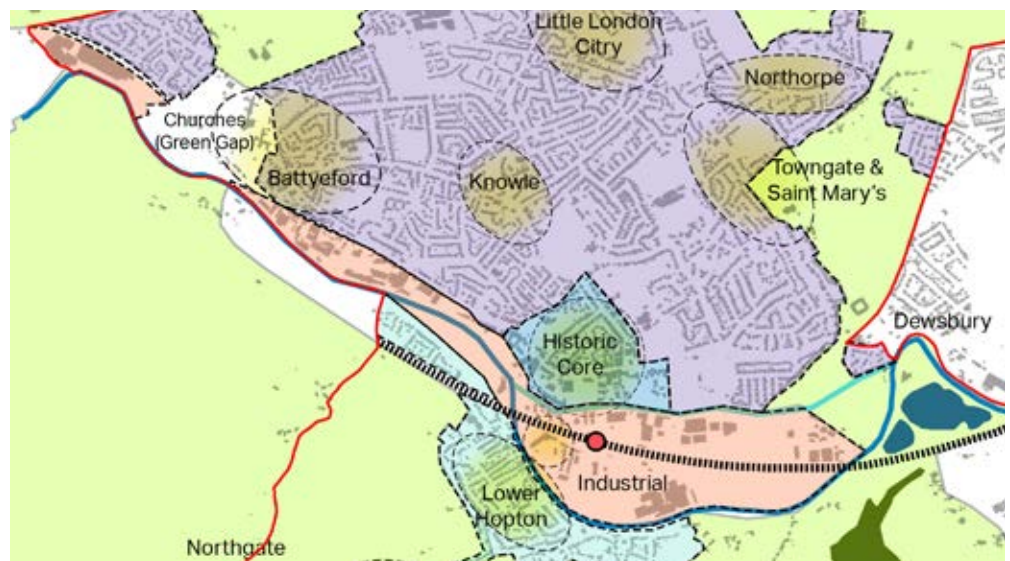


Figure 97: Character Area Code Locator



CV1 - CALDER VALLEY INDUSTRY / FLOOD PLAIN GUIDANCE

The Calder Valley acts as a “green & blue corridor”, due to its many linear features and extensive tracts of water. The presence of a navigable waterway and a history of boat building in Mirfield is an important asset to the landscape, although much of this is hidden from view. Reinforcing this green infrastructure corridor, prioritising sustainable travel methods and minimising flood risk impact through development should be prioritised within this area. A range of industrial uses are located here but a long term vision for change away polluting businesses and move towards sustainable, productive uses (like existing allotments) that are compatible with flood risk would help.

Regeneration Opportunity: Regeneration of brownfield land for productive uses; Green & blue infrastructure links; Employment uses; Flood plain compatible uses and defenses; Industrial building conversion; Transport orientated development (railway station); a masterplan vision could prevent piecemeal development of this long term opportunity;

Precedent schemes: Ledgard Wharf; Back Station Row; Mirfield Station underpass;

Plot boundaries: Rear boundaries masonry; Front: Low masonry with piers or drystone;

Built-form: Varies from large-scale industrial complexes to recently converted mills and townhouses closer to the railway station;

Building heights/ storeys: The industrial scale and massing sets a precedent in one off locations but the proximity to the train

station and town centre views requires a 3 storey limit to allow a reasonably high density on suitable brownfield sites;

Gardens and plot sizes: Regardless of the development opportunity small gardens or private outdoor space (balconies/ roof terraces) should be provided to ensure amenity access;

Development pattern: Due to the flat nature of the area a grid based on linking desire lines across the area can be set out as needed to accommodate uses;

Street alignment: A legible grid of connected routes for pedestrians and cyclists must be maintained and linked to bridge crossings and the canal towpath;

Building line formality: This area has potential to have a variety of character with formal streets close to the station, town centre or Lower Hopton and more informal areas feathering out eastward;

Corner sites: Legibility and orientation are key on this floodplain area of limited development, so where routes intersect placemaking features - be they planting, buildings, public art or signage are key;

Building scale and character: Scale should respond to neighbouring areas and key infrastructure / corridors; e.g. town centre, canalside and riverside;

Building materials: Stone, slate, brick metal and glass will suffice and some types of industrial/agricultural cladding may be explored but mostly materials harmony in with the local landscape;

Typical details and features: Simple industrial / waterfront details and stone construction or expressed framework.

A photograph of a stone staircase and wall. The staircase is made of large, dark grey stone steps. The wall is made of light-colored, rectangular stone blocks. A small, dark, rectangular window is set into the wall. A small plant with pink flowers is growing from the base of the wall. The background is dark and out of focus.

6. Next Steps

- Designating additional Conservation Areas
- Public referendum on Article 4 Directions
- Calder Valley Regeneration Masterplan
- Town square & public realm design
- Shopfront & signage design guide

Conservation area designation

An area has to be identified by the local authority as having a definite architectural quality or historic interest to merit designation (NPPF paragraph 127).

Conservation areas are normally designated by the local planning authority. The Secretary of State can also designate a conservation area anywhere in England in exceptional circumstances – usually where the area is of more than local interest.

The idea of recognising further areas within the Mirfield Parish as such cannot be done directly by the neighbourhood group but this document is an evidence base that can contribute towards the process should it be desired, e.g. around the town centre and historic urban core of Mirfield.

Article 4 Directions in conservation areas

Local Planning Authorities may remove “permitted development” rights by way of an Article 4 Direction. An Article 4(2) Direction is designed to prevent the special character of the area being spoilt by an accumulation of relatively minor but potentially inappropriate alterations to buildings and their immediate surroundings. Such alterations would usually constitute “permitted development” and would not require express planning permission.

The aim of the controls is to protect the dwelling houses from development likely to detract from the appearance of the conservation area in which homes lie.

There is scope in Upper Hopton Conservation Area for the creation of Article 4 Directions, however there have been a large number of alterations to buildings throughout the village such as changing wooden windows and doors

to UPVC equivalents. The removal of “permitted development” rights may include controls on changing windows and doors from wood to UPVC, changing the roof materials from natural traditional materials to artificial alternatives, alterations to boundary walls, gate piers, fences and gates, cladding or painting the exterior of the property.

However, the proposal of Article 4 Directions in Upper Hopton will require a further public consultation. It must be recognised that this can be costly and inconvenient to residents undertaking routine maintenance and repair.

Calder valley regeneration masterplan

It is suggested that a masterplan for the Calder Valley floodplain / industrial area could be useful to create a vision and manage change across this area. This could help the area transition from more polluting industries to the more sustainable forms of green industries and linked infrastructures that serve the town.

Town centre public realm scheme

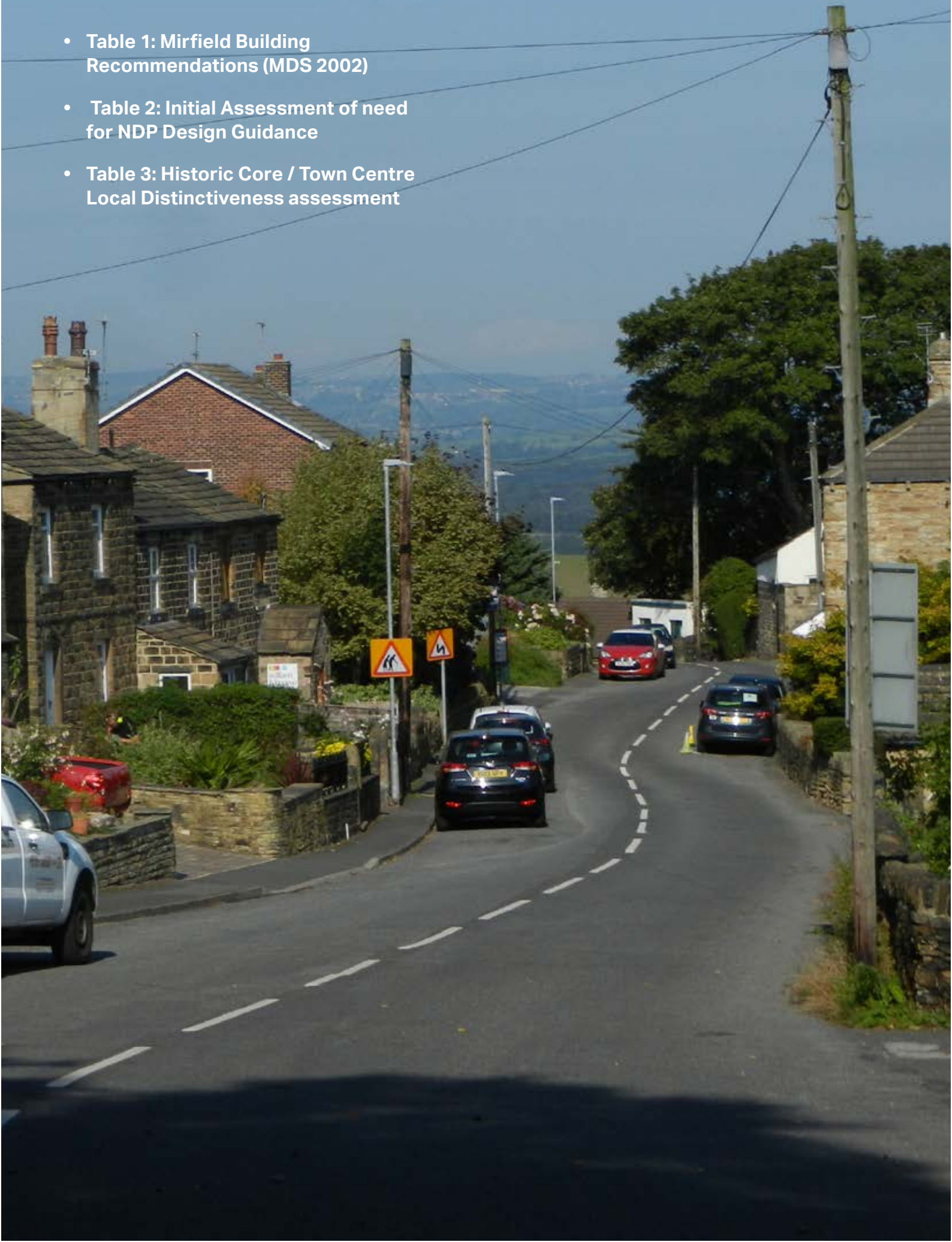
There are small but significant opportunities within the town centre to improve the space for pedestrians, cyclists and wheelchair users. These will not disrupt traffic flow, but balance it to create more space, landscaping and comfort for non-vehicle users in their own town centre.

Shopfront and signage design guide

One of the key ways to improve the attractiveness and competitiveness of the town centre is through simple rules to improve shopfronts. This may be best applied to a wider range of centres than just Mirfield, possibly addressed at the district level.

APPENDIX A.1

- Table 1: Mirfield Building Recommendations (MDS 2002)
- Table 2: Initial Assessment of need for NDP Design Guidance
- Table 3: Historic Core / Town Centre Local Distinctiveness assessment



MIRFIELD BUILDING RECOMMENDATIONS

(Mirfield Design Statement 2002)

Developments should maintain and strengthen visual cohesion and help to renew the specific architectural traditions of the area. Current and future developments should meet the following guidelines: -

- 1. Refer to local settlement patterns in layout.*
- 2. Ideally avoid substantial repetition of one house type.*
- 3. Respect good historic local characteristics and context of the particular site.*
- 4. Respond to typical setting and garden forms, having regard to surroundings and local vernacular.*
- 5. Refer to local buildings and proportion. (There will be a variety of proportions throughout the area and developments must reflect those which are adjacent)*
- 6. Refer to local distinctive details and materials and accurately match these to the chosen building form and adjacent buildings.*
- 7. Encourage reuse of appropriate building materials.*
- 8. Encourage energy efficiency.*
- 9. Any new or currently unimplemented planning applications should be discussed in detail with the Town Council, if they are of an unusual nature or give cause for concern.*
- 10. Whenever possible, new development should incorporate boundary walls and hedges, and not be laid out in an open plan style, so as to provide a unifying element to the village, in keeping with the style of surrounding properties.*
- 11. Existing buildings and features of architectural or historical importance should be retained whenever possible.*
- 12. Provision should be made in any new development to renovate existing buildings that contribute to maintaining the character of the area and by working with the original style where possible.*
- 13. Buildings should be maintained and extended using original materials and details (or new materials in keeping with the original ones), and in a style and proportion in keeping with the original property.*
- 14. Any retail building should have lighting and signage of a subdued and discreet nature. They should be non-reflective and not in very bright colours.*
- 15. Any housing development should aim to include either garaging or off-street parking. New garages should not obscure houses from the road whenever possible.*
- 16. Plans of how developments would fit in to their surroundings may be helpful. Developers should be encouraged to do this, providing the maximum amount of information and detail. Also, developers of infill sites, those closely neighboured by existing houses, should be prepared to provide additional material such as perspective drawings, to show how their development would appear in relation to their surroundings.*
- 17. Future developments should have an engineering survey undertaken.*
- 18. New developments must protect and enhance the external view of the town.*
- 19. Lateral extensions to buildings should not impede existing views of the local landmarks and should retain gaps between buildings.*
- 20. The insensitive choice of masonry paint colours can easily change the character of buildings making them too dominant in the streetscape.*
- 21. Resist new advertisement hordings and seek removal of existing hoardings and fly posting.*

Figure 98: Design Guidance and Code need appraisal table

		No. Heritage Assets / Local distinctiveness	Character quality / consistency	Opportunity for improvement	Risk of erosion / miss opportunity	Design Codes	Design Guidance	Planning Policy
LOCATION	Town Centre (Historic Core)	Strong (urban)	Strong (urban)	Medium: <ul style="list-style-type: none"> Specific Buildings Shopfronts Public Realm 	X - Higher	Yes	Yes	* Potential Conservation Area
	Urban (Riverside)	Medium	Strong	Medium: <ul style="list-style-type: none"> Riverside frontage Public Realm 	⊖ - Medium	Selected	Medium	Kirklees Residential Design Guide
	Suburban Neighbourhood & Local Centres	Patchy (focus areas)	Low / Mixed	Medium: <ul style="list-style-type: none"> (small infill sites) Local Centre areas 	⊖ - Lower (already suburbanised)	No	Selected	Kirklees Residential Design Guide
	Industrial (Valley bottom)	Strong	Low / Mixed High potential	High (regeneration)	X - High (opportunity)	No	Yes	* Potential Masterplan SPD?
	Conservation Area (Hillside)	Strong	Strong	Lower: <ul style="list-style-type: none"> Public Realm & Conservation 	⊖ - Lower (Conservation Area protection)	Yes	Medium	Existing Conservation Area
	Semi-rural (Greenbelt)	High (landscape)	Strong (Landscape)	Low: Semi-rural / Local distinctiveness	X - Mixed (Green Belt but major sites)	Low	Yes	Existing Green Belt
STREETS & SPACES	Open Spaces	High	Mixed / Medium	Medium	High	No / Existing	No / Existing	Policy
	Main streets	Medium	Mixed / Medium	Medium	Low	Medium	Specific	See: Kirklees Residential Design Guide
	Minor Streets	Mixed	Mixed / Low	Medium	Medium	Low	Low	See: Kirklees Residential Design Guide
	Edge of settlement	Mixed	Strong: Landscape/ views	Mixed	Low (Greenbelt)	Medium	Medium	See: Existing Town Boundary



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